

**TOWN OF LEESBURG
NOTICE OF TOWN COUNCIL PUBLIC HEARING
TO CONSIDER SPECIAL EXCEPTION APPLICATION
TLSE-2012-0005
OAKLAWN GAS STATION**

Pursuant to Sections 15.2-1427, 15.2-2204, 15.2-2205 and 15.2-2285 of the Code of Virginia, 1950, as amended, the **Leesburg Town Council** will hold a public hearing on **Tuesday, February 26, 2013 at 7:30 p.m.** in the Town Council Chambers, 25 West Market Street, Leesburg, Virginia, 20176 to consider Special Exception Application TLSE-2012-0005, Oaklawn Gas Station.

The subject property is vacant land located at 501, 524, and 534 Trimble Plaza SE. in the Oaklawn at Stratford Planned Development Community. The proposed lot is 1.3 acres in size located within the part of the site zoned PEC, Planned Employment Center. The property is identified as Loudoun County Parcel Identification Numbers (PINs) 233-20-0550, 233-29-6350, 233-29-9822. The *Town Plan* designates this property as “Regional Office” on the Land Use Policy Map.

Special Exception Application TLSE 2012-0005 is a request to allow a service station, convenience store and car wash in the PEC zoning district. The application includes a proposed 3,525 square foot convenience store, an 840 square foot car wash, and a 3,465 square foot outdoor canopy covering 8 fueling stations (16 pumps). The Applicant is requesting one modification of TLZO sec. 9.3.24 for the canopy height use standard.

Additional information and copies of this application are available at the Department of Planning and Zoning located on the second floor of the Leesburg Town Hall, 25 West Market Street, Leesburg, Virginia 20176 during normal business hours (Monday-Friday, 8:30 a.m. to 5:00 p.m.), or by contacting Irish Grandfield, Senior Planner, at 703-771-2766 or igrandfield@leesburgva.gov.

At these hearings, all persons desiring to express their views concerning these matters will be heard. Persons requiring special accommodations at the Town Council meeting should contact the Clerk of Council at (703) 771-2733 three days in advance of the meeting. For TTY/TDD service, use the Virginia Relay Center by dialing 711.

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Date of Council Meeting: February 26, 2013

**TOWN OF LEESBURG
TOWN COUNCIL PUBLIC HEARING MEETING**

Subject: TLSE-2012-0005, Oaklawn Gas Station, a special exception application to allow a service station, convenience store and car wash in the PEC zoning district.

Staff Contact: Irish Grandfield, AICP, Senior Planner, Dept. of Planning & Zoning.

Recommendation: Staff recommends that Council request the applicant redesign the site to emphasize the convenience store as part of the streetscape, to provide safe and convenient pedestrian access, and to better screen parking areas, the dumpster, and car wash.

Issue: Should a special exception be granted to allow a service station, convenience store and car wash in Land Bay C of the Oaklawn at Stratford planned development?

Fiscal Analysis: Approval of this application will generate business tax revenue to the Town. There are currently nine gas stations in Leesburg. The average assessed value of these properties is \$1,162,433, yielding \$2,267 in Town real estate taxes (and \$14,356 in County real estate taxes). The average amount of BPOL received from these nine businesses in 2012 was \$7,955 (including car wash and repair services). Gas taxes are also obtained, with local gas taxes slated to help fund the Dulles Corridor Metrorail Project known as the Silver Line.

Background: The applicant, Oaklawn Development Partners LLC is requesting Special Exception approval for a convenience store, fueling stations, and car wash in the PEC zoning district on 1.3 acres in Land Bay C of the Oaklawn site in south Leesburg. The application includes a proposed 3,525 square foot convenience store, an 840 square foot car wash, and a 3,465 square foot outdoor canopy covering 8 fueling stations (16 pumps). The Applicant is requesting one modification of TLZO sec. 9.3.24 for the canopy height use standard.

The subject property is vacant land located at 501, 524, and 534 Trimble Plaza SE. The proposed lot is 1.3 acres in size located within the part of the site zoned PEC, Planned Employment Center. The *Town Plan* designates this property as "Regional Office" on the Land Use Policy Map. The site is subject to proffers including a commitment to conformance with the H-2, Historic Corridor Architectural Control Overlay District Design Guidelines (TLZM-2005-0002, proffer #V.20).

Planning Commission Public Hearing and Recommendation: On January 17, 2013 the Planning Commission held a public hearing on the proposal and recommended denial by a vote of 6-1. No members of the public spoke at the public hearing. The Planning Commission noted that this application had changed very little from the one previously submitted in 2008 that was subsequently withdrawn following a Planning Commission recommendation of denial. The reason for a recommendation of denial in 2008 remains generally the same with this current proposal: the site layout is inconsistent with the Town's design guidelines. Planning Commission concerns relate to site layout including design consistent with the Town Plan and H-2 District,

pedestrian access and safety, and visibility of the car wash and gas pump canopy from nearby roads. The Commissioner who voted against the motion to recommend denial did so based on the need for additional gas stations in Town specifically the southeast portion of town, the opinion that site layout and pedestrian access were less important in a car-oriented environment, and the fact that no other gas stations in Leesburg were designed differently than this proposed one. Commissioners did not question the need for, or the benefit of, a gas station at this location but suggest that it seems possible for the site to be redesigned to meet Town design issues as well as operational needs.

The Planning Commission also discussed pedestrian access and safety from nearby areas. The Commission noted the draw of the proposed convenience store on the residential neighborhoods of Oaklawn. Staff agrees. As shown on the Figure 1 below, residential homes are within walking distance (800 feet) to the store. The site layout should anticipate pedestrians coming to the convenience store and it should facilitate their safe access to the store.

As currently laid out, one can question whether access to the store from Brown Roan Drive is as safe and convenient as possible (see Figure 2). While crosswalks are provided at the intersection of Brown Roan and Miller Drives, there is no sidewalk proposed from that intersection on the east side of Trimble Plaza leading to the store. Staff did not recommend the sidewalk on the east side of Trimble Plaza because of the conflict it creates for pedestrians and vehicles at the main driveway entrance to the service station. As discussed later in this report, staff proposes a redesign of the site layout that should improve pedestrian access and safety.

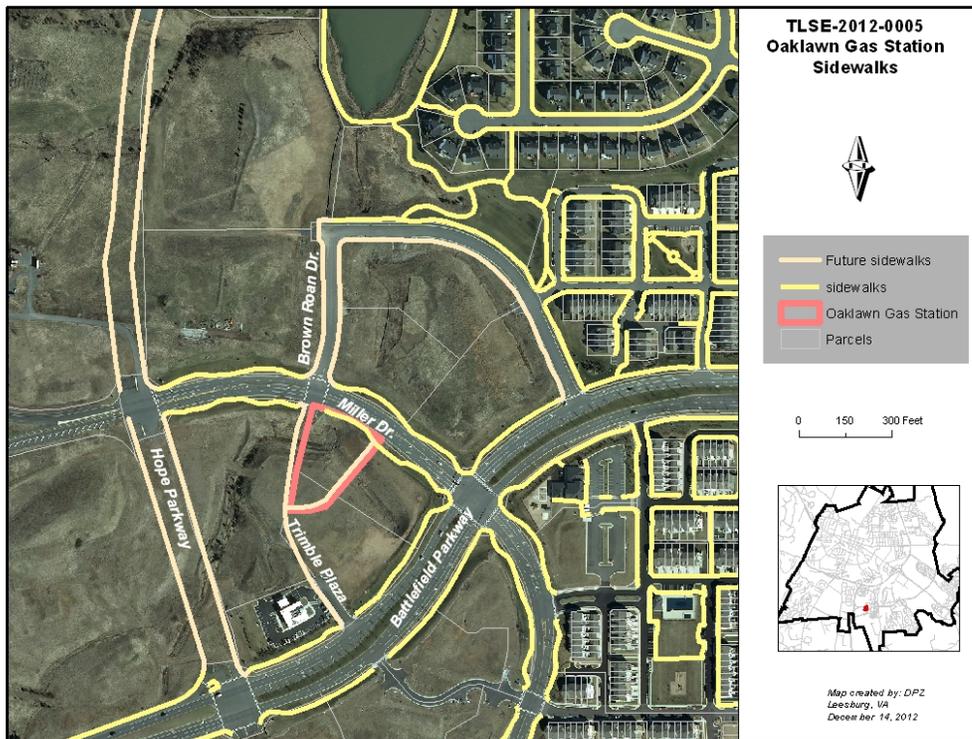


Figure 1. Regional Sidewalks

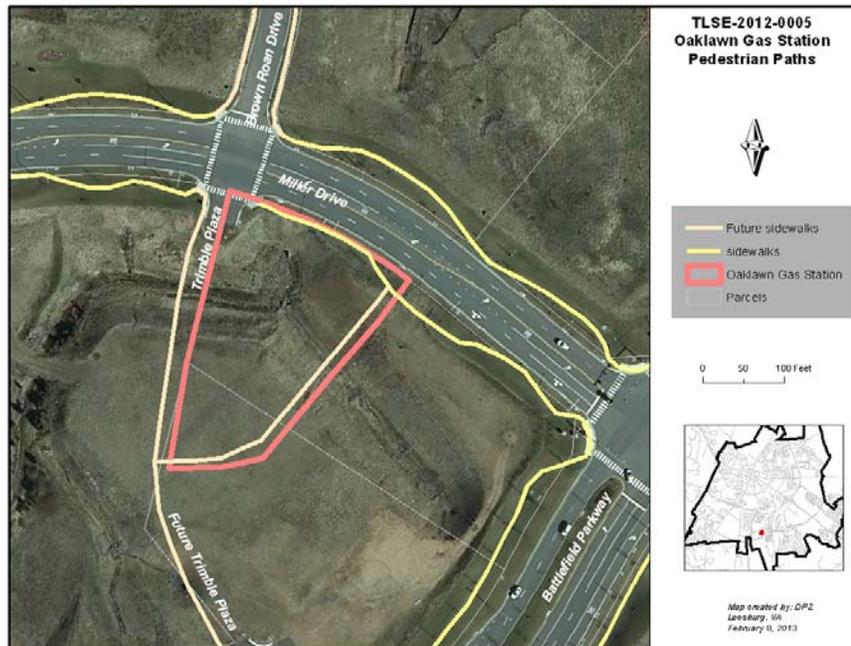


Figure 2. Pedestrian Pathways at the Site

Staff Analysis: The Town Plan, approved rezoning concept for this site, Town staff, and the Planning Commission all support the use of this site for a gas station, convenience store, and car wash. No one questions the need for the facility or the fiscal benefits of the convenience store and gas pumps. The issue is whether it is possible to have a well functioning service station which is designed to be more consistent with Leesburg’s community character and design principles. Staff’s research suggests that the facility can develop and function as envisioned by the Town Plan and H-2 Design guidelines. Principally, staff notes the following to support this conclusion:

1. Examples of convenience store and gas station designs around the country exhibit the design principles expressed in the Town Plan and H-2 Design Guidelines.
2. The nature of the site as a “green field” rather than infill development. The proposed lot has not been recorded so there is flexibility available with which to create a site layout that meets design objectives.
3. The fact that the Applicant has provided no alternative design analysis to demonstrate that the design guidelines cannot be met more completely.

Staff believes (and the Planning Commission supports the position) that community character does not need be sacrificed for economic development; the two goals are not mutually exclusive. This is not an “either-or” situation. In fact, there are case studies that show just the opposite – dedication to community design goals through attention to design detail will cumulatively contribute to community character which in turn leads to further economic development.

Staff finds that the proposed site layout does not meet the Gateway Corridor and H-2 design principles expressed in the plan (Town Plan, Regional Office Site Design and Location Criteria, p. 6-25). Staff believes it is possible to meet both site design and the operational objectives of the

service station owner as evidenced by other similar gas station/convenience store developments around the nation. There are outstanding issues with the proposal related to the convenience store's relationship to the street; site access and pedestrian convenience; and parking location related to the front of store.

The following discusses how the site could be redesigned to meet the Town Plan and H-2 guidelines and the benefits of these design principles. Additionally, staff has included an example below of how the site plan could be slightly adjusted to accommodate these design principles.

Design Principles

- 1. The convenience store should have an architectural presence on the street and provide direct pedestrian access that does not conflict with vehicles.**

Recommended Change to Oaklawn Plan:

- Move the car wash and associated vehicle stacking that block direct pedestrian access to convenience store to the better screened area near Miller Drive.
- Reorient the convenience store to engage the adjacent street (Trimble Plaza).



Example of a convenience store/gas station with safe pedestrian access

Benefits:

- Safe and convenient pedestrian access.
- Safe vehicle travel circulation pattern.
- Town-style layout (as opposed to suburban) enhances community character and meets Town design guidelines.



Example of car wash and pumps screened by convenience store

- 2. Separate the gas pumps and car wash from pedestrian paths and provide screening of these facilities from street view.**

Recommended Change to Oaklawn Plan:

- Move the car wash to the area near Miller Drive where there is landscaping to screen the facility.

Benefits:

- Car wash is screened.
- Safer pedestrian access.
- Simplified vehicle circulation.
- Improved visibility of car wash for

employee.

3. Screen the parking from view of the primary street.

Recommended Change to Oaklawn Plan:

- Relocate parking to side and rear of the convenience store.

Benefits:

- Enhanced streetscape.
- Improved view-scape.
- Further separates pedestrians from vehicle traffic.



Example of convenience store parking screened by building

4. Reduce conflicts between loading, trash service, and pedestrians.

Recommended Change to Oaklawn Plan:

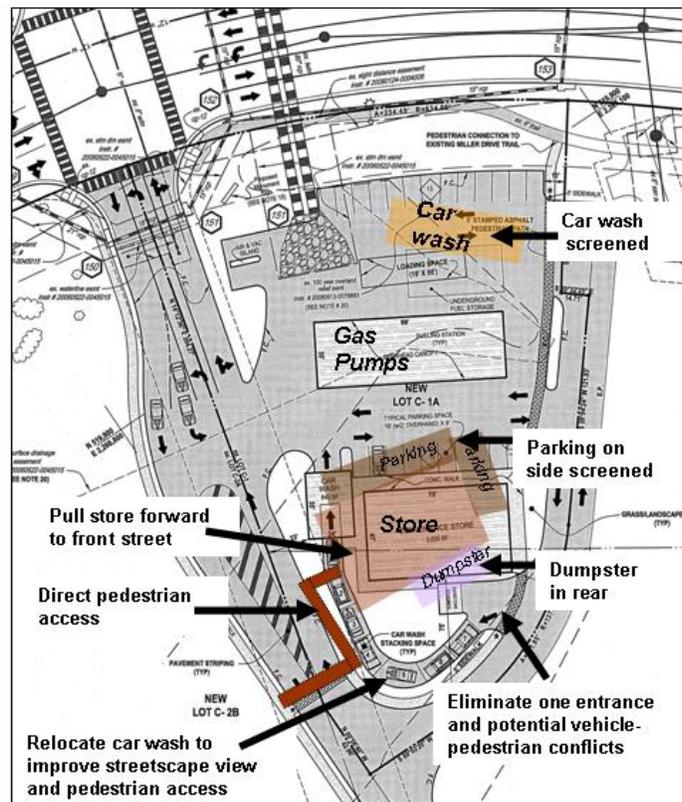
- Relocate the trash dumpster and loading area away from pedestrian travel paths and car wash stacking.

Benefits:

- Improved safety.

Staff Recommendation:

Staff recommends Town Council defer the application to allow time for staff and the applicant to work together on a site design that will better meet Town Plan and H-2 design guidance and addresses the applicant's operational objectives. To the right is an example site layout that could address safety and design concerns by providing a pedestrian entrance on the street; screening the car wash, parking areas, and gas pumps; and simplifying the vehicular movements onsite.



Conditions of Approval and Resolution: Should the Town Council find it appropriate to recommend approval of this application, staff suggests the following minimum conditions:

1. Substantial Conformance. This special exception shall be in substantial conformance with the special exception plat entitled “Oaklawn-Commercial Land Bay C”, Sheets 1-8, revised through 12-31-2012 and prepared by Paciulli Simmons & Associates.
2. No Waivers Expressed or Implied. With the exception of the modification listed below, approval of this special exception does not express or imply any waiver or modification of the requirements set forth in the Subdivision and Land Development Regulations, the Zoning Ordinance, or the Design and Construction Standards Manual, except as may have been approved as part of this application. Final plats, site plans, and construction drawings are subject to applicable town regulations.
3. Modification Granted. Modification to TLZO 9.3.24F (Canopy Height) is hereby granted as follows:

“Canopy height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14’0” except as necessary to allow the canopy to remain level where the finished grade of the land slopes beneath the canopy. The final grade on the land beneath the canopy shall not slope more than a total of three and one-half (3.5) vertical feet. The height of the canopy shall not exceed 3’3” above the lower edge of the canopy fascia at any one point and the maximum height of the canopy at any one point shall not exceed 20’9.””

4. Mechanical Equipment. All mechanical equipment, including roof-top equipment, shall be screened from public view by appropriate architectural elements.
5. Building Elevations. The structures shall be approved by the Board of Architectural Review (BAR) in accordance with Proffer #V.20 of #ZM-159.
6. Building Facades. All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, coloring and detailing.
7. Traffic Signage. The applicant shall install a “Do Not Block Intersection” sign for northbound traffic on Trimble Way prior to the main entrance to the gas station.
8. Car Wash Façade. Provide one or more windows on the building elevation of the car wash facing Trimble Way to better define the streetscape and break up the mass of the building.

A Resolution is also included as attachment 4.

Attachments

1. Planning Commission Staff Report dated January 17, 2013
2. Written Statement of Justification
3. Special Exception Plat
4. Resolution

II. CURRENT SITE CONDITIONS

The subject property is vacant land located at 501, 524, and 534 Trimble Plaza SE. in the Oaklawn at Stratford Planned Development Community. The proposed lot is 1.3 acres in size located within the part of the site zoned PEC, Planned Employment Center (see figure 2). The property is identified as Loudoun County Parcel Identification Numbers (PINs) 233-20-0550, 233-29-6350, 233-29-9822. The *Town Plan* designates this property as “Regional Office” on the Land Use Policy Map.

Miller Drive provides the public road access to this site. Trimble Plaza is a private travelway that is proposed to connect Miller Drive to Battlefield Parkway through Land Bay C. The southernmost portion of Trimble Plaza (adjacent to Battlefield Parkway) was constructed as part of development of the Northwest Federal Credit Union. The remainder of Land Bay C is vacant although there has been some preliminary grading of the site.

The proposed 1.3 acre site is comprised of portions of three separate parcels in Land Bay C. Should this special exception application be approved, the applicant will submit a revised subdivision plan to record this lot.

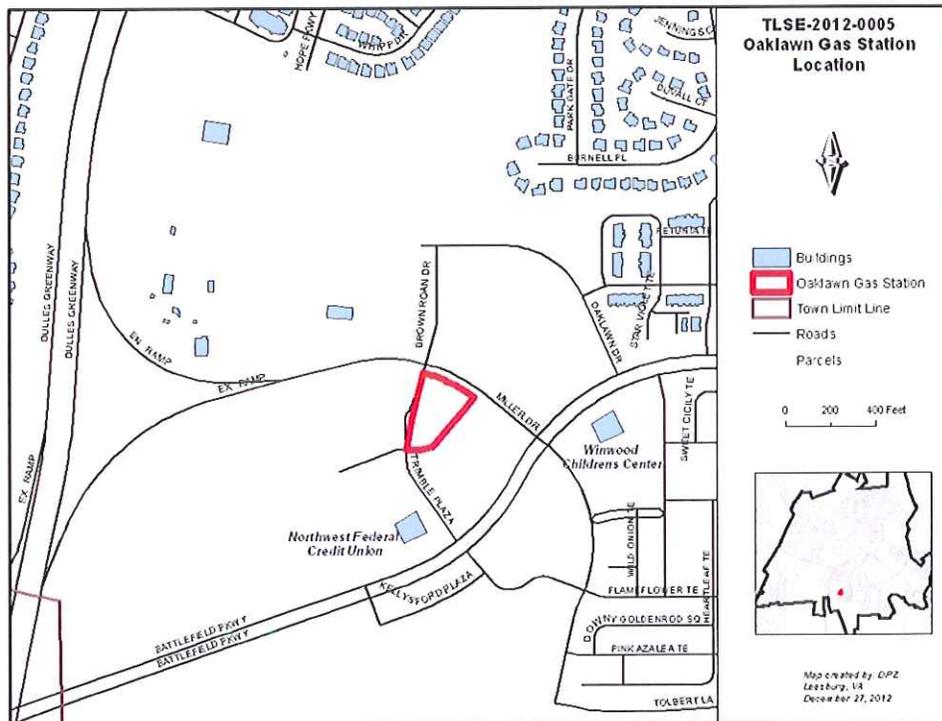


Figure 2. Location

III. ZONING HISTORY

The site is currently zoned PEC (“Planned Employment Center” - see figure 3). In June 2003 as approved in ZM-159 the 448 acre Oaklawn at Stratford planned development was rezoned to PRC (Planned Residential Community) and PEC (Planned Employment Center). In January 2006 a Town Plan Amendment changed the land use designation for Land Bay C and three other land bays from Community Office /Light Industrial to Regional Office. In February of 2007 a Rezoning Concept Plan Amendment and Proffer Amendment (application number TLZM-2005-0002) were approved to permit by special exception an automobile service station with a convenience store and/or car wash as an alternative use in Land Bay C. The amendment only changed the potential location for the service station; the size of the facility was retained as approved in the initial rezoning.

The applicant previously submitted a special exception for a gas station in Land Bay C in 2008 (TLSE-2008-0001). That application was subsequently withdrawn following a Planning Commission public hearing on July 17, 2008. No members of the public spoke at the hearing. The Planning Commission discussed issues related to Regional Office design, the Gateway Corridor designation in the Town Plan and the H-2 guidelines. Concerns were expressed regarding pedestrian access, multiple points of entry to the site, and visibility of the canopy and pumps from Miller Drive. The Commission also questioned the architectural design. The commission then voted to recommend denial of the special exception request on a 4-1-2 vote. Rather than moving forward for Town Council action, the applicant withdrew the special exception request.

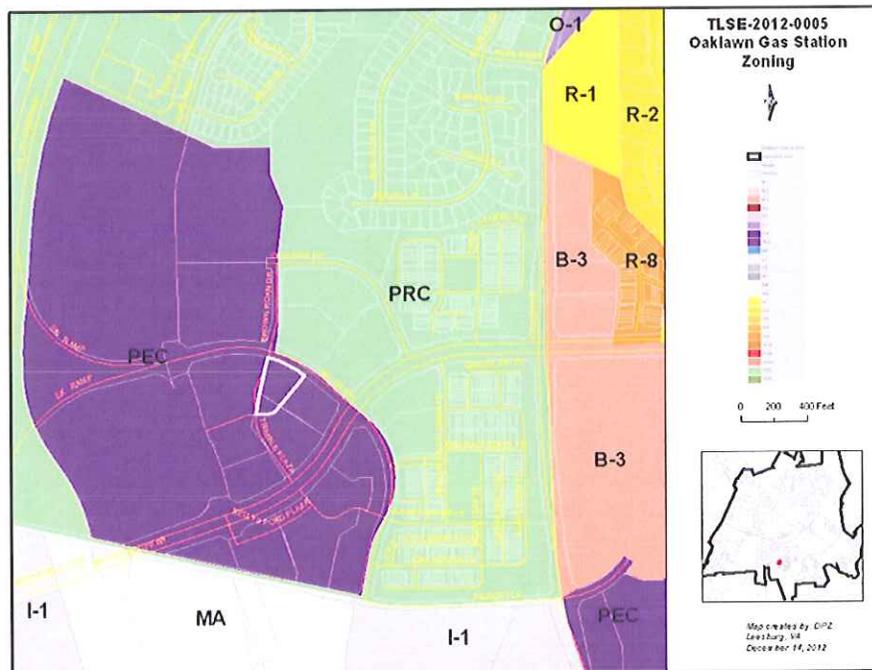


Figure 3. Zoning

IV. STAFF ANALYSIS

A. **Town Plan Compliance:** The Town of Leesburg Zoning Ordinance (TLZO) Section 3.4.12 requires an assessment of how the proposed special exception use will comply with applicable provisions of the *Town Plan*.

1. **Proposed Use:** The property is designated “Regional Office” (see figure 4). The Town Plan describes regional office as designed for a broad spectrum of local and regional employment as well as provision of daily retail and service needs of people associated with the primary office use in a manner that reduces automobile uses (Intent Statements #1 and 2, page 6-24). The “Uses” section for Regional Office does not specifically mention a gas station, convenience store, and car wash as appropriate (Town Plan, p. 6-25). However, in 2007 the Town approved TLZM-2005-0002, a concept plan and proffer amendment allowing the gas station, car wash, and convenience store in Land Bay C as support services to the primary office use. For this reason, staff considers the proposed uses acceptable as related to the Town Plan.

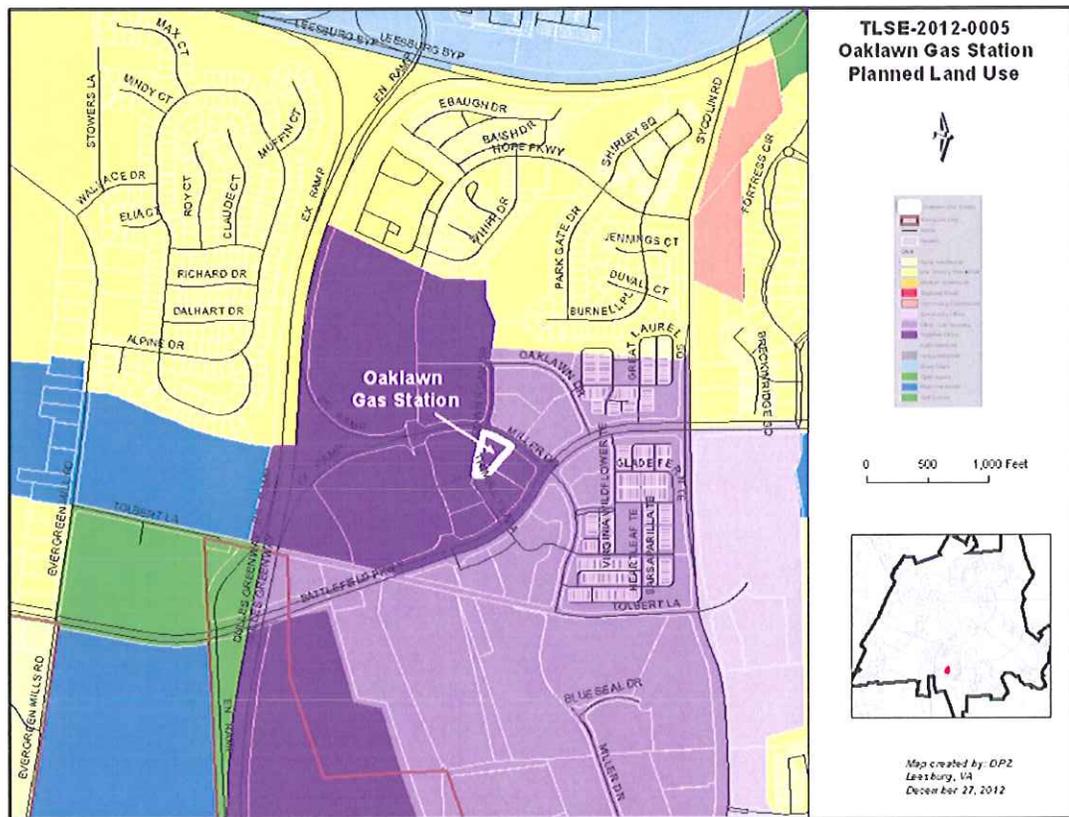


Figure 4. Planned Land Use

- 2. Gateway Corridor:** The Town Plan indicates that Regional Office development should be compatible with Leesburg's character as established by the more traditional urban core (Intent Statement #5, page 6-24). The proposed design has the rear of the convenience store facing Trimble Way with the large overhead canopy being the most visible element from the street. This is a conventional suburban gas station design rather than traditional town design expected for a Gateway Corridor in a development planned as Regional Office. Staff's assessment is that the site layout does not meet the Town Plan Gateway Corridor guidance.

The applicant has responded that the proposed use is auxiliary to the primary regional office park uses and requires special site design considerations. They argue that the service station's layout is dictated by its operational needs.

Staff has provided the applicant with examples of existing development where this type of use is developed in a manner that is more consistent with Gateway Design corridors (see example below in Figure 5). However, the applicant has stated they are unwilling to alter the site layout. They have indicated that their experience with service stations shows that for operational ease the car wash and pumps must both be easily accessible to employees from the convenience store in the manner proposed by their design. Staff notes there are numerous examples locally and nationally of alternative designs including ones where the car wash is not adjacent to the convenience store (see Figure 6 as an example).

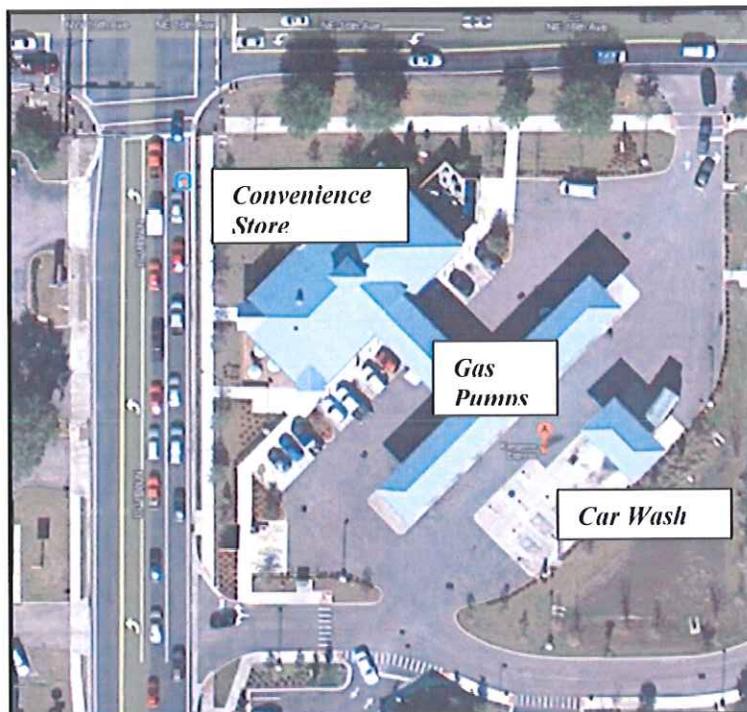


Figure 5. Alternative Site Layout Example



Figure 6. Gas Station in Leesburg showing separate car wash

3. **Site Design:** The Town Plan states “site design should conform to H-2 district design guidelines” (Site Design and Location Criteria #6, p. 6-25). Staff’s assessment of the site design indicates that this does not meet H-2 design guidelines for building size, scale, and orientation nor for building appearance. While building appearance is addressed at the time of site plan, the size, scale, and orientation of the building must be addressed now during the special exception process (otherwise, a site plan would not be in “substantial conformance” with the approved special exception plat). A more thorough discussion of the H-2 District Design issues is provided under item “B” of this Staff Analysis section of this report. An example of an alternative site layout that meets H-2 Guidelines is shown in figure 5.

4. **Staff Findings Related to the Town Plan:** Both in the review of the similar 2008 application and for this special exception, staff has consistently identified the siting and design issues as not conforming to the Town Plan guidance. Staff requested that the applicant reconfigure the site layout so that the convenience store engaged one of the adjacent roads (Trimble Way or Miller Drive) and relocate the car wash to a less visible area of the site. Although the applicant has never provided proof of consideration of alternative designs, they maintain that their operational needs prevent them from meeting the design policies. Staff does not believe operational objectives and site design guidelines are mutually exclusive; it is possible to achieve both. No evidence has been provided to show operations can not work in harmony with design guidelines. In summary, the proposal is not in conformance with the Town Plan design guidelines and staff has seen nothing

that suggests that site design as prescribed by the Town Plan and H-2 Design guidelines cannot be achieved while also meeting operational needs.

- B. H-2 District Design Guidelines:** The Town Plan guidance is not the only directive citing H-2 guidelines. The applicant's proffers also commit this site to review for conformance with the H-2, Historic Corridor Architectural Control Overlay District Design Guidelines (TLZM-2005-0002, proffer #V.20).

Staff has identified several areas in conflict with the proffered H-2 Corridor Design Guidelines including, but not limited to, the siting of the buildings, the massing and height, pedestrian access and screening (see attachment 4) further described below.

1. ***Building Orientation:*** The "siting/relationship to roadway" guidelines for H-2 indicate that the building should be oriented toward and have direct pedestrian access to the roadway from which it is accessed (in this case, Trimble Way). Primary buildings should not be placed behind parking facing the entry. With the current site plan, the car wash could be relocated toward the landscape buffered part of the site adjacent to Miller Drive with appropriate architectural treatments. As well, parking areas should not be located exclusively at the front of building sites.

The Applicant has responded that the site layout is a customary configuration for the proposed uses of convenience store, gas pumps and car wash. They indicate that the operational demands require employees based in the convenience store to be able to see the gas pumps as well as being proximate to the car wash building. Staff acknowledges that the H-2 guidelines do not fit the particular operational desires of the applicant, but the applicant has provided no compelling reason for not conforming to the guidelines.

2. ***Site Access:*** (H-2 Corridor Design Guidelines, pp. 32-33): To minimize traffic conflicts, vehicular entrances/exits to a site should be consolidated to a single location. Further, "entrances and driveways should permit safe and convenient pedestrian crossing where they intersect sidewalks and other pedestrian circulation systems." In this case, the proposal has three two-way access points on a small lot creating conflicts with pedestrians who will be walking to this site and to other future uses in this Land Bay. Fewer entrances and more uninterrupted sidewalks would better achieve the H-2 design guidelines or the design goals of Regional Office.
3. ***Pedestrian Safety:*** H-2 guidelines call for safe and convenient pedestrian access. The pedestrian access as proposed from across Trimble Plaza is awkward, long and wraps around the rear of the building requiring pedestrians to cross the 35-foot wide car wash entrance aisle. Staff has recommended an alternate site layout with the convenience store facing Trimble Way. That alternative site layout would facilitate direct, safe, and convenient pedestrian access.

4. **Parking:** Parking for this site is exclusively at the front of the site inconsistent with the parking lot guidance for the H-2 District. (H-2 Corridor Design Guidelines, pp. 34). Again, an alternative site layout could address this issue but the applicant has expressed an unwillingness to make the change due to operational desires.
5. **Building Appearance:** Because they face private internal travelways that function as roads, the side and rear facades of the convenience store should be designed with as much attention as the primary façade. (*Note: This can be addressed during review of the actual Certificate of Appropriateness and is not required at this time.*)

Board of Architectural Comments

The Board of Architectural Review (BAR) reviewed this project for consistency with the H-2 guidelines on November 19, 2012. Some members of the BAR expressed concern with the site layout as proposed. However, the BAR stopped short of providing any specific site layout changes. Other comments were made regarding the architecture, but architectural comments will be dealt with later as part of the actual Certificate of Appropriateness review.

In summary, given the proposed site layout's nonconformance with the design guidelines described above, staff finds the proposal does not meet the H-2 standards as of TLZO 7.6.7 nor the H-2 Design Guidelines.

- C. **Use/Compatibility with Adjacent Properties:** The site is located internal to a largely vacant area of a planned development community as such there are no incompatibility issues (see table below).

Direction	Existing Zoning	Current Use
North	PEC/PRC	Vacant
South	PEC	Vacant
East	PEC	Vacant
West	PEC	Vacant

- D. **Transportation:** A conditional waiver of the traffic study requirement was granted due to the fact that there is an adequate road network to handle the vehicle trips generated solely by this use. Site ingress and egress will occur from private travelways within Landbay C; no direct access will occur from the site onto Miller Drive or Battlefield Parkway. As such, the existing public roads are adequate to handle traffic generated by the proposed gas station, convenience store, and car wash.

However, staff notes that there are similarities between the proposed major access point for this gas station site (proposed adjacent to Miller Drive) and the existing Sheetz gas station located adjacent to Edwards Ferry Road. In each of these examples, a two-way private travelway with a single lane in each direction and a minimal left turn lane into the site serves a gas station. There have been repeated

stacking and access issues for vehicles at the Sheetz station. While there are sufficient road improvements now for the proposed gas station facility here at Oaklawn, staff is concerned that another "Sheets" situation (i.e. excessive stacking in the public right of way) could occur in the future as this site is fully developed.

One way to avoid this possibility is for the land bay to be served by two lanes into the site from Miller Drive (onto Trimble Plaza) with the innermost lane dropping off as a left turn only lane into the proposed gas station. The outer lane would then enhance internal vehicular circulation by serving as a bypass lane for access to the rest of the landbay which would minimize the turning movement conflicts that potentially could cause excessive stacking in the Miller Drive public right of way. This need for this type of improvement will be further analyzed in a future traffic study to be provided as the rest of the land bay develops.

On the positive side, this site (unlike Sheetz) has more than one point of access to the landbay. In the future, there will be three points of ingress/egress to the landbay (Trimble Plaza connects to Battlefield Parkway and a future extension of Hope Boulevard will also lead to Landbay C).

The Traffic Study Waiver issued on June 14, 2012 (see Attachment 5) finds that the subject intersection will meet acceptable levels of service for the proposed service station/car wash/convenience store uses. To provide an opportunity to address this situation in the future should it arise, the traffic study waiver granted for this case contained a condition that states "Future land development within Landbay C will impact this intersection [Miller Drive/Trimble Plaza]" and that as a result the Town will require a traffic study for future development proposals beyond the subject service station/car wash/convenience store within Landbay C.

E. Utilities: The Department of Utilities has no principal objections to the applications at this time but noted that there are comments that must be addressed at the time of site plan review. (Attachment 8 from Deputy Director of Utilities).

V. Specific Use Standards per Zoning Ordinance: The Zoning Ordinance (TLZO) provides specific use standards for convenience stores, service stations and car wash uses under Article 9 below.

Section 9.3.3 Car Washes

A. *"Car washes shall be located and designed so that vehicular circulation shall not conflict with traffic movements in adjacent streets, service drives, driveways and/or parking areas."* - Staff believes the present design with a separate entrance from off-site from the convenience store and gas pumps to the car wash and three two-way entrances and exits elsewhere on site is not optimal and could be redesigned to reduce potential conflicts. To enter the car wash a vehicle must drive off-site into an adjacent travelway and then reenter the site at a fourth entrance, this one exclusive for the car wash. Future development of the remainder of the Land Bay will only

increase the potential for conflicts between vehicles moving around the site.

- B. *Car washes (as an accessory use) shall have no separate and exclusive curb cut access to the abutting roadway.* – The Applicant’s proposal technically complies with this standard but there is an exclusive entrance into the site for the car wash from an off-site travelway.

Section 9.3.8 Convenience Food Stores

- A. *No convenience food store shall be located on a lot adjacent to residential dwelling units.* - Applicant’s proposal complies with this standard.
- B. *All business services, storage and display of goods shall be conducted within a completely enclosed building.* - Applicant’s proposal complies with this standard.

Section 9.3.24 Service Stations

- A. *Service stations shall not include ancillary uses such as vehicular or tool rental (including moving vans) and shall be limited to the servicing of vehicles and non-automotive accessory retail sales of snacks, convenience foods, and similar products.* - Applicant’s proposal complies with this standard. Applicant does not propose any service bays.
- B. *Service stations shall not include an outdoor storage area for more than three (3) abandoned, wrecked or inoperable vehicles on the site for more than one week, subject to the limitation that there shall be no dismantling, wrecking, or sale of said vehicles or part(s) thereof.* - Applicant’s proposal complies with this standard.
- C. *Outside sales and display areas shall be shown on the plan to be reviewed by the Planning Commission and Town Council.* - Applicant proposes no outside sales or display areas.
- D. *The parking of commercial vehicles shall be prohibited on the site of a service station. This provision shall not be interpreted as prohibiting the parking of vehicles actively engaged in delivering fuel or other supplies to the service station.* - Applicant shall comply with this standard.
- E. *No permit shall be issued for a service station if the entrance or exit for vehicles is:*
 - 1. *In the same block front within 200 feet of any school, public playground, hospital, church, or public library;* - Applicant’s proposal complies with this standard.
 - 2. *If such entrance or exit is located within 20 feet of an “R” District within the same block front;* - Applicant’s proposal complies with this standard.

3. *No permit shall be issued for a service station if any part of any structure, including underground gasoline tanks or service aisles, is located within 100 feet of any building or grounds of a school, public playground, hospital, church, or public library or any "R" District.* - Applicant's proposal complies with this standard.

F. *Canopy height measured from the finished grade to the lowest point on the canopy fascia shall not exceed 14'0". The overall height of canopies shall not exceed 17'3".* - The Applicant has submitted a modification request for this standard (see discussion under "Modifications.")

G. *Canopy lighting shall be fully recessed in the ceiling of the canopy.* - Applicant's proposal complies with this standard.

H. *All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, coloring and detailing.* - Final design of the buildings must be approved by the Board of Architectural Review under the H-2 Corridor Design Guidelines.

VI. Site Design:

Parking. Convenience Store: 6 per 1,000 square feet gross floor area (21 total)
Car Wash: 1.0 per employee, plus required stacking spaces(0)
Fuel Islands (Service Station): 1 per employee(0)

21 parking spaces are required plus 7 car wash stacking spaces.
23 parking spaces are provided per the special exception plat.

Stacking Spaces. TLZO Section 11.8.2 requires a minimum of seven (7) stacking spaces per car wash bay. This proposal meets this standard.

Architectural Elevations. The applicant will need to obtain Board of Architectural Review (BAR) approval pursuant to proffer #V.20 of #ZM-159.

Landscaping/Buffering. A 12.5-foot wide buffer with an S-2 Screen is provided along the frontage of Battlefield Parkway in accordance with the with the rezoning concept plan requirements of #ZM-159. Submission and review of the final site plan application will finalize arrangement and selection of plant materials. The applicant shall provide interior and exterior parking lot landscaping as required by ordinance.

Lighting. Site and Canopy lighting is subject to review and conformance with the DCSM. Foot candles at the property shall not exceed .5 and the source of illumination shall not be visible. Canopy lights are recessed per TLZO 9.3.21.G

to prevent them from being visible offsite. The B.A.R. can also address this issue as part of its review of this proposal.

Storm Water Management. Storm Water Management and Best Management Practices for storm water quality must be in accordance with Loudoun County and Town of Leesburg criteria in force at the time of site plan approval. DPR staff have indicated that due to the proposed gas station use the site is a “BMP Hotspot.” Stormwater management issues will be addressed at the time of site plan.

Hours of Operation. The hours of operation for this facility are intended to be 24 hours a day, seven (7) days a week.

Signs. Applicant will provide signage in accordance with the requirements of TLZO Article 15 Signs.

Pedestrian/Vehicular Accessibility. Applicant provides pedestrian walkways along three sides of the site with the western side served by a sidewalk across the entrance drive on an adjacent parcel. Pedestrian access to the convenience store from Trimble Way is awkward as proposed due to the location of the car wash. Staff requested an alternative design to address this issue but the applicant is unwilling to alter the site layout at this time.

VII. Modifications

Request: Pursuant to TLZO 3.4.13, the applicant requests a modification by the Town Council of TLZO 9.3.24.F a use standard for the canopy height at service stations. This use standard states that “*Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".* The applicant is requesting canopy height as measured from the average finished grade to the lowest point on the canopy fascia not exceed 17'0" while the overall height of the canopy not exceed 20'6".

The applicant’s justification states that the increase is necessary as a safety factor so that semi-tractor trailers may pass beneath the canopy. Conceptual grading plans have the site sloping downward to the east with a difference of up to 3' 6". Because the Zoning Ordinance states that height is measured from the average (emphasis added) grade around the entire structure, the canopy will need a greater height at the lower end in order to remain level. Having an appropriate canopy height with adequate clearances is necessary as a safety factor in the operation of the service station.

Modification Standards: TLZO Section 3.4.13 states that the criteria for evaluation of a use standard modification request is whether Town Council determines that the modification is “necessary to accommodate reasonable use of the site.”

Staff Analysis and Recommendation: The maximum legal truck height is 13'6". The use standard that a canopy fascia be at least 14'0" in height is in place to keep the fascia relatively low while allowing trucks to pass safely underneath. Staff agrees that an increase in the minimum canopy fascia height is needed onsite based on the proposed grades in order to provide a safety factor for trucks. Staff also agrees that some modification of overall height is warranted to accommodate reasonable use of the site given the proposed grades (although staff would encourage the applicant to have a less severe grade under the canopy).

Staff recommends the following modification to Zoning Ordinance Use Standard TLZO 9.3.24F for the purpose of safety:

"Canopy height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0" except as necessary to allow the canopy to remain level where the finished grade of the land slopes beneath the canopy. The final grade on the land beneath the canopy shall not slope more than a total of three and one-half (3.5) vertical feet. The height of the canopy shall not exceed 3'3" above the lower edge of the canopy fascia at any one point and the maximum height of the canopy at any one point shall not exceed 20'9"."

VIII. Proffers:

The site is subject to the applicable proffers of TLZM-2005-0002 Oaklawn (and amendment to the original #ZM-159 Oaklawn case). The Applicant is seeking special exception approval. As a result, the proffers can not be amended through this process. However, conditions to mitigate adverse impacts of the proposed use may be imposed by Council in accordance with TLZO Sec. 3.4.14.

IX. Analysis and Recommendation:

Staff has provided examples of alternative site layouts that could meet both the design objectives and operational needs. The applicant has not demonstrated that they have considered alternative site designs. Nor have they shown how the design guidelines prohibit successful operation of a service station, convenience store, and car wash. For these reasons, staff recommends **denial** of Special Exception TLSE-2012-0005 because the application does not comply with the following:

- Town Plan Regional Office criteria specifying that site design should comply with H-2 District Design Guidelines.
- Regional Office Gateway Corridor design criteria in the Town Plan.

Should the Planning Commission find it appropriate to recommend approval of this application, staff suggests the following minimum conditions:

1. Substantial Conformance. This special exception shall be in substantial conformance with the special exception plat entitled "Oaklawn-Commercial Land Bay C", Sheets 1-8, revised through 12-31-2012 and prepared by Paciulli Simmons & Associates.
2. No Waivers Expressed or Implied. With the exception of the modification listed below, approval of this special exception does not express or imply any waiver or modification of the requirements set forth in the Subdivision and Land Development Regulations, the Zoning Ordinance, or the Design and Construction Standards Manual, except as may have been approved as part of this application. Final plats, site plans, and construction drawings are subject to applicable town regulations.
3. Modification Granted. Modification to TLZO 9.3.24F (Canopy Height) is hereby granted as follows:

"Canopy height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0" except as necessary to allow the canopy to remain level where the finished grade of the land slopes beneath the canopy. The final grade on the land beneath the canopy shall not slope more than a total of three and one-half (3.5) vertical feet. The height of the canopy shall not exceed 3'3" above the lower edge of the canopy fascia at any one point and the maximum height of the canopy at any one point shall not exceed 20'9."

4. Mechanical Equipment. All mechanical equipment, including roof-top equipment, shall be screened from public view by appropriate architectural elements.
5. Building Elevations. The structures shall be by the Board of Architectural Review (BAR) in accordance with Proffer #V.20 of #ZM-159.
6. Building Facades. All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, coloring and detailing.
7. Traffic Signage The applicant shall install a "Do Not Block Intersection" sign for northbound traffic on Trimble Way prior to the main entrance to the gas station.

X. Sample Planning Commission Draft Motions

Denial

Special Exception Use

I move that Special Exception Use Application TLSE-2012-0005, Oaklawn Gas Station, be forwarded to the Town Council with a recommendation of denial on the basis that the Approval Criteria of Zoning Ordinance Section 3.4.12 have not been satisfied for the following:

- Town Plan Regional Office criteria specifying that site design should comply with H-2 District Design Guidelines.
- Regional Office Gateway Corridor design criteria in the Town Plan.

Approval

Special Exception Use

I move that Special Exception Use Application TLSE-2012-0005, Oaklawn Gas Station, be forwarded to the Town Council with a recommendation of approval subject to the conditions of approval contained in the Staff Report dated January 17, 2013 on the basis that the Approval Criteria of Zoning Ordinance Section 3.4.12 have been satisfied and that the proposal would serve the public necessity, convenience, general welfare and good zoning practice; and further recommend approval of the following modifications:

- A modification of TLZO 9.3.24F (Canopy Height) to allow the maximum height of the canopy at any one point to not exceed 20'9."

Approval, with revised conditions

Special Exception Use

I move that Special Exception Use Application TLSE-2012-0005, Oaklawn Gas Station, be forwarded to the Town Council with a recommendation of approval subject to the conditions of approval contained in the Staff Report dated January 17, 2013, as amended by the Planning Commission on January 17, 2013, on the basis that the Approval Criteria of Zoning Ordinance Section 3.4.12 have been satisfied and that the proposal would serve the public necessity, convenience, general welfare and good zoning practice; and further recommend approval of the following modifications:

- A modification of TLZO 9.3.24F (Canopy Height) to allow the maximum height of the canopy at any one point to not exceed 20'9."

Attachments

1. Page 1 Special Exception Plat
2. Written Statement of Justification
3. Modification Request
4. Preservation Planner/BAR memo dated November 21, 2012
5. Conditional Traffic Study Waiver dated June 14, 2012
6. Department of Plan Review memo dated November 20, 2012
7. DPZ Zoning referral memo dated November 19, 2012
8. Department of Public Works memo dated November 19, 2012
9. Utilities memo dated November 1, 2012

Oaklawn, Landbay C
Special Exception Application for an
Automobile Service Station, Convenience Store and Car Wash
Written Statement
April 3, 2012
Revised October 12, 2012

I. Introduction

Oaklawn Development Partners, LLC is filing a special exception application for an automobile service station, convenience store and car wash on 1.15 acre tract of land within Land Bay C of the Oaklawn Planned Employment Community (PEC) development. The 1.15 acres currently is part of 3 separate parcels that will be consolidated into one parcel subsequent to the approval of the proposed special exception application. The Oaklawn PEC district is subject to the proffers for ZM #159 and TLZM 2005-0002. The Oaklawn PEC has been planned for two service stations within the community; however, the proffers for TLZM 2005-0002 stipulated that a special exception would be required if one the of the service stations were to locate in Land Bay C.

II. Type of Operation

A service station is proposed with eight fueling stations containing 16 gas pumps. Along with the service station will be a 3,525 square foot convenience food store and an 825 square foot car wash.

III. Hours of Operation

This facility will operate 24 hours a day, seven days a week.

IV. Traffic Impacts

The traffic studies submitted for ZM #159 and TLZM 2005-0002 analyzed the proposed special exception use. The most recent traffic study analyzed a gas station in Land Bay C as one of the two gas stations within the Oaklawn development. Oaklawn has proffered and phased road improvements based on this study. The impacts of the proposed special exception uses were accounted for in these prior studies and in the proffered and phased road improvements.

V. Impacts on adjacent uses and measures proposed to mitigate such impacts.

The governing Oaklawn PEC zoning with concept plan and proffers created a comprehensively planned community that accounted for a variety of uses needed to create a thriving employment center. Two gas stations always have been contemplated within Oaklawn. In Land Bay C, the proposed special exception site is surrounded by future uses that could include office, light industrial, data center, free-standing restaurant, fast food with drive through and bank with drive-through. One of the Mixed-Use Center land bays is located across Miller Drive from the proposed special exception uses, which is

planned for similar uses proposed in this application, as well as office and residential uses. Oaklawn has been planned with a unified landscaping plan along the public streets, and the special exception uses will comply with the unified landscaping plan along Miller Drive. The uses internal to Land Bay C are intended to be integrated with one another using a coordinated private driveway network.

VI. Compliance with Section 3.4.12 Approval Criteria.

A. The proposed use will not adversely affect the use of neighboring properties.

An amendment to TLZM 2005-0002 in 2007 relocated the 2nd gas station from MUC 2 to Land Bay C for the purpose of moving it further from the residential units in Oaklawn. This proposed Special Exception fulfills the commitment of that proffer amendment. As for adjacent uses, the gas use is fully compatible with the other planned uses such as office, retail, bank, restaurant, pharmacy and personal service.

B. The use shall comply with applicable zoning district regulations and applicable provisions of the adopted *Town Plan*, including but not limited to the Plan's Land Use Compatibility policies.

Land Bay C within Oaklawn is designated for regional office use on the Planned Land Use Map of the Town Plan. Oaklawn is a planned employment community for which the concept plan accommodated locations for the primary uses, as well as the supporting uses. The concept plan for Land Bay C, in particular, designated restaurant, bank and service station uses along with the office uses as part of this regional office community. The regional office designation contemplates the need for retail and service uses for the daily needs of the workers, customers and businesses in the development. This special exception use proposes to provide such uses as contemplated in the concept plan for Oaklawn. Development within Oaklawn is subject to the H-2 Corridor Design Guidelines to ensure architectural compatibility among the various uses within Oaklawn.

C. The location, size and height of buildings, structures, walls and fences, and the nature and extent of screening, buffering and landscaping shall be such that the use will not hinder or discourage the appropriate development and use of adjacent or nearby land and/or buildings.

The proposed uses have been planned as part of the other Land Bay C uses, which include an office building, restaurant and pharmacy uses surrounding the proposed uses. The internal private street network accommodates these uses, and none of the uses interfere with each other's operations. The proposed buildings will be one-story, and the service station canopy will comply with the Zoning Ordinance maximum height requirement. Oaklawn has a unified landscape plan bordering all of the public streets within Oaklawn. The site containing the proposed uses

fronts on Miller Drive, and the Miller Drive landscaping will be provided in accordance with the required buffer and the Oaklawn landscape plan. A six-foot wide trail also will be constructed within the setback along Miller Drive as part of the Oaklawn trail system.

D. The proposed use will be such that pedestrian and vehicular traffic generated will not be hazardous or conflict with the existing and anticipated traffic in the neighborhood and on the streets serving the site.

Both the vehicular and pedestrian access is part of the networks planned for the Oaklawn planned development. The proposed special exception uses have been contemplated as part of the Oaklawn planned development from its inception; therefore, the traffic generated by these uses was anticipated in the initial planning for Oaklawn.

VII. Compliance with Section 9.3, Use Standards

A. Section 9.3.3 Car Washes

A. Car washes shall be located and designed so that vehicular circulation shall not conflict with traffic movements in adjacent streets, service drives, driveways and or parking areas.

The car wash entrance is accessed from an internal driveway that serves this use. The vehicles exiting the car wash will use the site entrance that accesses the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Car washes (as an accessory use) shall have no separate and exclusive curb cut access to the abutting roadway.

The proposed accessory car wash use has an access point from a driveway and vehicles exiting the car wash will use the site's entrance onto the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Section 9.3.6 Convenience Food Store

A. No convenience food store shall be located on a lot adjacent to residential dwelling units.

There are no dwelling units located on the lots adjacent to the proposed special site.

C. All business services, storage and display of goods shall be conducted within a completely enclosed building.

The applicant shall adhere to this requirement once the convenience food store is operating.

D. Section 9.3.21 Service Stations

- A.** *Service stations shall not include ancillary uses such as vehicular or tool rental (including moving vans) and shall be limited to the servicing of vehicles and nonautomotive accessory retail sales of snacks, convenience foods, and similar products.*

Vehicle servicing is not proposed with this application nor is vehicle rentals. Retail sales will occur as part of the convenience food store use.

- B.** *Service stations shall not include an outdoor storage area for more than three (3) abandoned, wrecked or inoperable vehicles on the site for more than one week, subject to the limitation that there shall be no dismantling, wrecking, or sale of said vehicles or part(s) thereof.*

There will be no inoperable vehicles, since servicing of vehicles is not proposed with this service station.

- C.** *Outside sales and display areas shall be shown on the plan to be reviewed by the Planning Commission and Town Council.*

Outside sales and display are not proposed with this use.

- D.** *The parking of commercial vehicles shall be prohibited on the site of a service station. This provision shall not be interpreted as prohibiting the parking of vehicles actively engaged in delivering fuel or other supplies to the service station.*

The applicant will comply with this requirement.

- E.** *No permit shall be issued for a service station if the entrance or exit for vehicles is*

1. *In the same block front within 200 feet of any school, public playground, hospital, church, or public library; or*

None of these uses are included in Oaklawn.

2. *If such entrance or exit is be located within 20 feet of an "R" District within the same block front.*

The entrance for the service station is internal to Land Bay C, which does not contain any R District zoning.

3. *No permit shall be issued for a service station if any part of any structure, including underground gasoline tanks or service aisles, is located within 100*

feet of any building or grounds of a school, public playground, hospital, church, or public library or any "R" District.

None of these uses exist within Oaklawn.

- F.** *Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".*

A modification request pursuant to Section 3.4.3 of the Zoning Ordinance has been submitted to modify the canopy height fascia not to exceed 17'0" and the overall height not to exceed 20'6".

- G.** *Canopy lighting shall be fully recessed in the ceiling of the canopy.*

The proposed canopy lighting will comply with this requirement.

- H.** *All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, color and detailing.*

Oaklawn is subject to the H-2 Corridor Design Guidelines and process. Those guidelines and process can ensure that this provision will be adhered to by the proposed uses.

**TLSE 2012-0005
Oaklawn Commercial Landbay C
Automobile Service Station with Car Wash and Convenience Retail**

ZONING ORDINANCE MODIFICATION REQUEST

**October 12, 2012
Revised January 2, 2013**

The applicant respectfully requests the following zoning ordinance modifications:

Pursuant to Section 3.4.13, Compliance with Use Standards, a modification by the Town Council of Section 9.3.24.F, Automobile Service Station Canopy Height.

Zoning Ordinance Section to be Modified: Section 9.3.24 Use Standards – Service Stations F. Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".

Proposed Modification:

Canopy Height as measured from the average finished grade to the lowest point on the canopy fascia should not exceed 17'0". The overall height of the canopy shall not exceed 20'6".

Applicant's Justification:

The applicant has consulted with a gas station operator who recently constructed a service station in Ashburn and who likely will be the operator of this gas station. The proposed height is derived from adding a safety factor to the maximum legal truck height of 13'6". This safety factor is needed due to the proposed grading for the property and also due to the possibility of vehicles that might exceed the maximum height restriction. The site slopes downward to the east, which, based on preliminary grading, will be a difference of up to 3'6". The canopy will need a greater height at the lower end in order to remain level. Having an appropriate canopy height with adequate clearances is necessary as a safety factor in the operation of the service station.



LEESBURG BOARD OF ARCHITECTURAL REVIEW MEMO
FROM REGULAR MEETING ON 19 NOVEMBER 2012

To: Irish Grandfield, Environmental Planner
From: Kim K. Del Rance, LEED AP, Preservation Planner
Date: November 21, 2012
RE: Oaklawn Service Station on Land Bay C, Trimble Plaza and Miller Drive (PEC, H-2)

Preservation Planner comments:

This site is bounded by created boundary lines within a planned employment community at the intersection of Miller Drive, a public right of way and private roadways within the development. The proposed building architecture and materials are not compatible with the previously approved neighboring buildings within the development as was agreed to in proffers in ZM#159 and TLZM-2005-0002.

Areas in conflict with the agreed to H-2 Corridor Design Guidelines include, but are not limited to the siting of the buildings, the materials, the massing and height, pedestrian access and screening.

- ◆ This service station has been preceded by approvals for residential design (TLHP-2006-0020) and commercial designs (TLHP-2007-0009 Exxon and TLHP-2006-0112 Winwood Daycare) most of which have gabled roofs on brick structures with cornice detailing and substantial materials which were approved as being in character with historic Leesburg.
- ◆ This convenience store and car wash design shows an excessive amount of an insubstantial material, EIFS, with concrete block and brick- three textured materials and a flat roof with an exaggerated front parapet for signage. Each of these individually would be a candidate for change to be appropriate, but taken together the combination is not in accordance with the guidelines stated above.

P.14 Siting / Relationship to Roadway

*A publicly oriented building, such as an office or commercial facility, should have as its primary orientation the major roadway from which it is accessible. This orientation should be expressed not only in the building's predominant architectural expression but also through the presence of a clearly recognizable entrance. When the approach to such a building is from a different direction than that of the roadway, the **building should have an entrance which is oriented to the direction of approach**, while maintaining a consistent architectural identity that is also recognizable from the adjacent roadway.*

- ◆ The entrance from the main roadway (Miller Drive) is to an entrance from a private road on the west and east to the main convenience store building. The building is sited facing Miller Drive but is obscured by the canopy and gas pumps. Rotating the building to face the intersection and driveway entrance and away from the gas pumps is appropriate.

The side or rear façade of a building located adjacent to and visible from a roadway should be designed with as much attention as the primary façade, even if the roadway is not the building's primary orientation or point of entry.

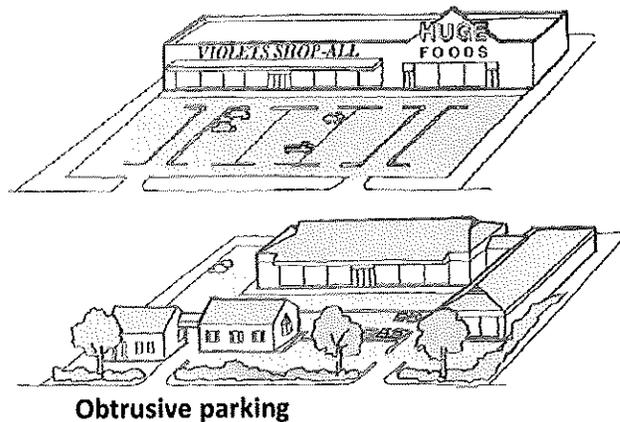
Approaching from the south is essentially facing the rear of the building, the car wash entrance and the dumpster enclosure. The sides of the convenience store are also blank and have no human scale detailing or screening. The car wash entrance is prominent as those entering to the west side will drive towards it as they pass on the west side to find the site entrance.

- ◆ The current siting demands that all facades have more of a presence or are screened appropriately.

P.18 Massing and Roof form

Projects containing many buildings or accommodating a variety of different functions generally should provide variety in building size and massing. A transition from small or low buildings on street frontages to larger and taller structures on the interior of the properties is generally encouraged.

An obtrusive expanse of parking occurs when a single large building mass is located at the rear of the site (below, left). Variety in building massing and siting promotes visual interest and reduces the visual impact of parking (below, right).



Elements such as roofs or canopies should not appear to be pasted on, but should be Three dimensional structural building elements such as ... parapet walls...and roofs should be integral parts of the building design and built of materials similar to or compatible with the rest of the structure.

- ◆ Façade appears to be “pasted on” the front and can be seen easily from the rear as not being functional.
- ◆ As can be seen by drawings of façade and gas pump canopies above, the façade will rise above the height of the canopies which will block its visibility and change the scale of the building in relation to the canopies which are already being requested to be higher than is normally allowed due to the slope of the site.

P.21 Materials and Detailing

The materials used in the construction of a building design can have significant implications for its appearance. Since the solid, exterior wall surface, for example, often accounts for up to 80% of a building's façade area, the selection of an appropriate wall material may be the single most important material choice. Yet, too often materials are selected that have no relationship either

to those found in the Leesburg area or with other materials chosen for the building itself. The following guidelines provide assistance in the selection, application and detailing of appropriate building materials:

- ◆ Artificial veneer materials such as Styrofoam-based simulated stucco, cast or fiberglass stone or brick and plastic appear insubstantial and unauthentic, and should be avoided on new buildings.
- ◆ Traditional roofing- materials that are also appropriate for new construction include slate, wood shakes, and standing seam metal such as copper, tin or aluminum.
- ◆ The history and nature of Leesburg indicate that wood, red brick and native stone are prevalent building materials; these materials likewise are appropriate for newer construction throughout town. Stucco+ and concrete masonry occur less frequently, but may be acceptable building materials in some instances, depending on a building's style and function.
- ◆ The specific materials used on a building or set of related buildings should be compatible with each other in terms of size, shape and texture. While variety in the size or shape of different building components may add interest to a building's exterior appearance, more than one highly textured material such as stone, ashlar-faced concrete block or wood shingled roofs should not be used on a single building. Instead, such materials should be combined with those that are simpler and less textured to avoid a confusing or overly elaborate appearance.
- ◆ While the selection of materials for new construction should be influenced by those materials that are familiar in the built environment of Leesburg, the architectural detailing of such materials-the way in which they are used in relation to other materials in the building- may be less traditional as long as it relates to the overall design of the building. The use of brick, split-faced concrete block and stucco or EIFS (which is used is unclear) all on this small building is too elaborate and the variety should be reduced to two or fewer of these materials.
- ◆ While this is only a second submission review and colors are not mentioned, P. 23 should be referred to by the applicant and designers before submitting an application for a Certificate of Appropriateness.

P. 25 Signage, P. 28 Screening, P.32 Site Access are all applicable to specific elements of this proposal

To minimize traffic conflicts, vehicular entrances and exits to a site should be consolidated at a single location a sufficient distance away from street intersections with high traffic volume.

- ◆ The main entrance follows this, but the two additional entrances on the east side of the site should be minimized.

The apparent width of entrances and driveways should be minimized wherever possible by the provision of a planted median strip of at least 6' in width between incoming and outgoing traffic particularly if two or more lanes are provided in each direction.

- ◆ All three entrances are wide and currently there is no planted median, only stripes which do not protect the pedestrians from oncoming traffic in either direction.

Entrances and driveways should permit safe and convenient pedestrian crossing where they intersect sidewalks and other pedestrian circulation systems.

- ◆ The current pedestrian access from across Trimble Plaza is awkward, long and wraps around the rear of the building requiring pedestrians to cross in front of the car wash entrance driveway as traffic leaves the roadway.

*Pedestrian access to sites should be provided from **all adjacent public sidewalks**, protected from vehicular traffic by grade separation, plant materials and/or berming.*

- ◆ Currently there are two one pedestrian accesses which have none of the above conditions.

Where appropriate, effort should be made to provide secondary pedestrian access to sites and linkages between adjacent sites by the provision of pedestrian paths not necessarily associated with public roadways.

- ◆ This site is a candidate for having additional pedestrian access since it is completely surrounded by roadways.

P.33 Parking Lots

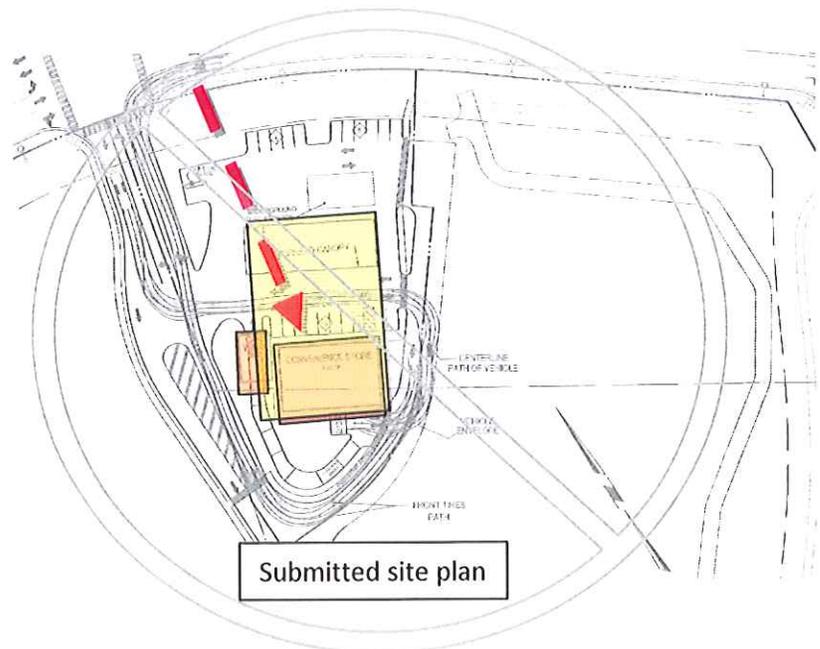
... it is the provision of on-site parking that most distinguishes the character of new and recent development

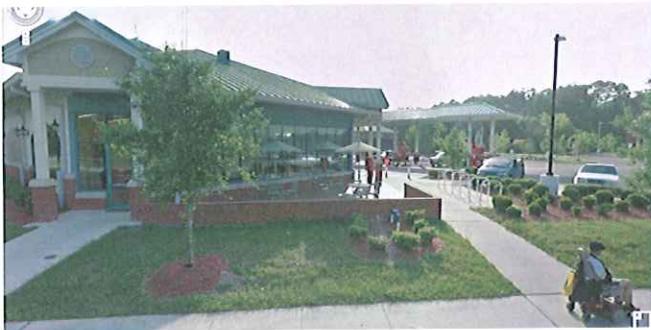
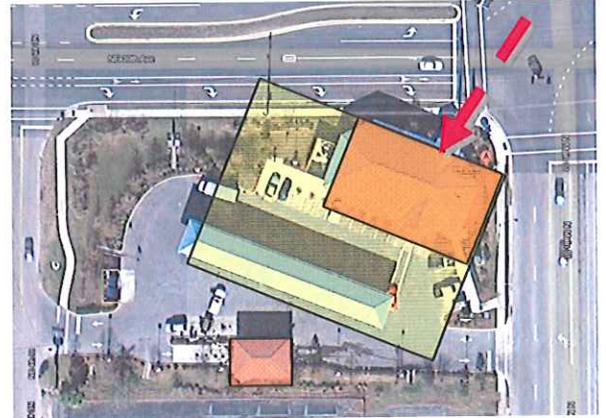
*A single large expanse of parking should be avoided. Instead, parking should be provided in smaller, well-defined areas, separated and screened by features such as berms, access drives, landscaping and/or buildings. Parking areas **should not** be located exclusively at the front of building sites.*

- ◆ Parking for this site is exclusively at the front of the site, which is **not to be done**.

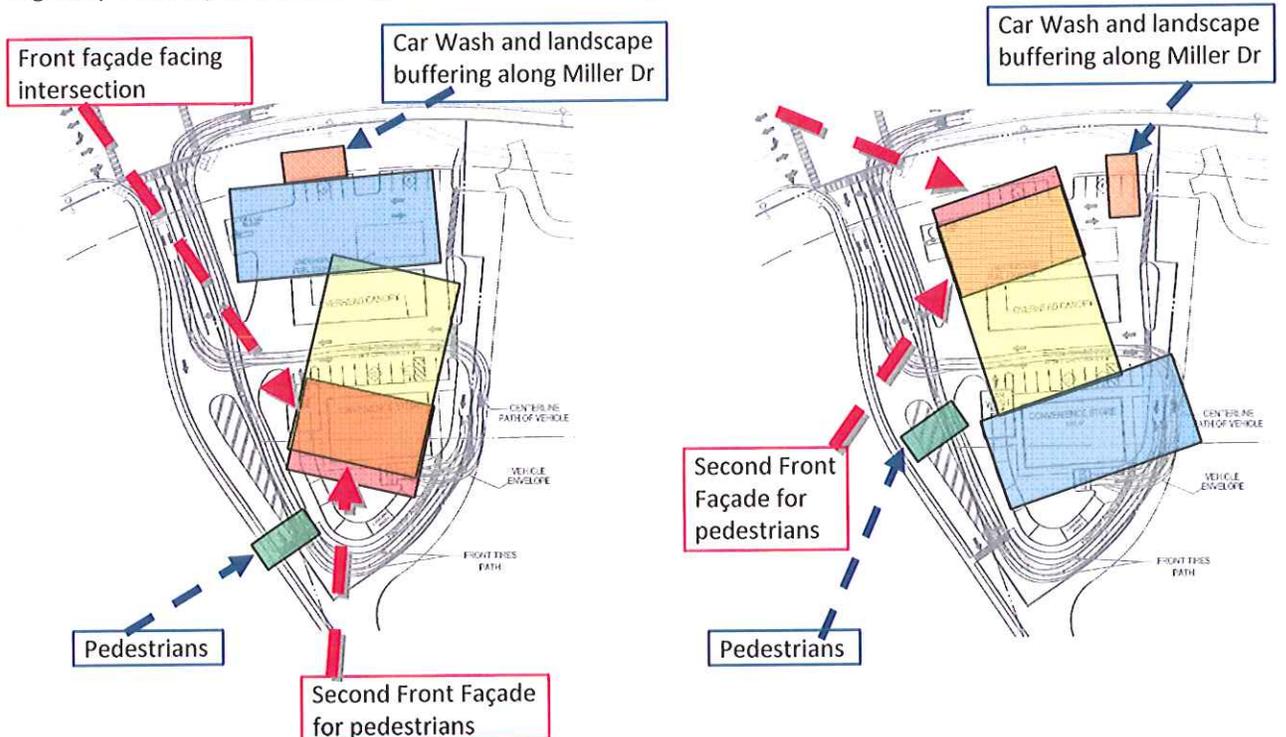
Wherever possible, some of the required parking should be provided at the sides or rear of the site, following the traditional pattern of downtown Leesburg. Promoting variety in the placement of buildings on the site (see Size and Scale, Chapter I/Building Design Standards) may also help achieve spatial variety in the placement of parking areas.

- ◆ This site should have alternative parking placement using the rear and/or sides of the building for smaller parking areas.





Above are two examples of a gas station, convenience store and car wash sited along two public roadways where the main building is facing the main intersection and has a pedestrian approach while maintaining views of the gas pumps from inside the store, allowing free movement through the parking lot for the store, the pumps and the car wash. The hipped roofs shown here are more appropriate than the flat roof with pasted on front façade which is more suited to a highway rest stop or a shopping center not in Leesburg, VA.



Above left is one example of a slight siting change by moving the car wash up to inside the berm and landscape buffer with proper screening and allowing safe pedestrian crossing at the SW corner that would meet the H-2 guidelines, but it is unknown if this would also meet all other site requirements.

Above right is an example of siting change that would meet the H-2 guidelines, but this is not possible as the site is currently bounded.

STAFF COMMENTS SUMMARY

There are alternative site placements that are appropriate to this site that will follow the agreed upon H-2 Corridor Guidelines. It is unclear at this point if the site boundaries as drawn can support an appropriately sited building, canopy and car wash. Other items such as size, scale, massing and height can be adjusted later, but building siting should be addressed at this stage. If new siting that meets the H-2 guidelines cannot be met with these boundaries, then staff recommends further site plan research and the appropriate site boundaries be established before approval of this use is considered.

BOARD OF ARCHITECTURAL REVIEW COMMENTS SUMMARY FROM NOVEMBER 19, 2012 BUSINESS MEETING

Several members of the board expressed issues with the building architecture. They agreed with staff that the architecture can be addressed at a later date and the security need for the gas pumps to be visible from the store is valid, but the site issues should be addressed by looking at alternative site arrangements and that a better site plan should be used.

Kim K. Del Rance, LEED AP
Preservation Planner



Department of Planning & Zoning

25 West Market Street ■ 20176 ■ 703-771-2765 ■ Fax: 703-771-2724 ■ www.leesburgva.gov

June 14, 2012

Oaklawn Development Partners, LLC
Attn: Mr. Andy Shuckra
P.O. Box 1573
Ashburn, VA 20146

RE: **Oaklawn Gas Station, Landbay C - Waiver Request for Traffic Study**
Location: Trimble Plaza, SE
PIN# : 233-20-0550, 233-29-6350 and 233-29-9822
Related Applications: ZM #159, TLZM 2005-0002, TLSE 2012-0005

Dear Mr. Shuckra:

Staff received your special exception application for a gas station proposed to be located in Landbay C along with your request, dated April 6, 2012, for a waiver of a traffic study. The following is my response to your request.

Ordinance Requirements. TLZO Sec. 3.4.6.F.3. **Submittal Requirements, Written statement, Traffic Impact Analysis** specifies that a traffic impact analysis be submitted with special exception applications. This analysis will include the maximum expected trip generation and the trip distribution by mode and time of day based on the current Institute of Transportation Engineers (ITE) Manual, the internal road network, and the connection into the existing transportation network. A traffic study is required if:

- The property is adjacent to a road that carries, or proposes to carry, more than 550 vehicle trips per day (VTD); or
- The property contains a road proposed to carry more than 500 VTD; or
- The property is adjacent to a road that currently or is proposed to carry 100 or more peak hour trips; or
- If the traffic study for the subject property is more than one (1) year old.

Your special exception application would trigger all four of the criteria listed above.

Waiver and Modification Standards. TLZO Sec. 3.4.6. F.3 (e). provides for the option to waive the aforementioned requirements. These standards state in pertinent part that the requirements may be waived at the discretion of the Land Development Official in response to a written request from the applicant when filed with the initial application. Waivers may be granted upon finding that a traffic analysis is unnecessary due to:

TLSE 2012-0005, Oaklawn Gas Station
Planning Commission Public Hearing Staff Report
January 17, 2013
Page 31

Oaklawn Special Exception for Gas Station – Landbay C
Response to Request for Waiver of Traffic Study
6/14/2012
Page 2 of 2

- existence of previous studies and analysis which adequately cover the extent of the proposed development and impacts; or
- receipt of adequate proof from the applicant that the proposed use does not increase the number of employees or customers using the site.

Analysis and Response. Staff notes that traffic counts had been done within the past year at the intersection of Miller Drive and Brown Roan Drive. In a memo received from Michael Armstrong, Engineer with the Department of Public Works, dated April 24, 2012 (Attachment 1) it was noted that review of these counts and consideration of the proposed development showed that the subject intersection would meet acceptable levels of service with the service station/car wash/convenience store uses. Therefore, the waiver requirement for the existence of a previous study which adequately covers the extent of the proposed development (a service station/car wash/convenience store) has been met. As such, in the opinion of the Transportation Division, the traffic study could be waived based on the following two conditions:

1. Future land development within Landbay C will impact this intersection and that as a result the Town will require a traffic study for future development proposals beyond the subject service station/car wash/convenience store within Landbay C.
2. As a part of this special exception application review, the applicant must demonstrate how WB-50 and SU vehicles will adequately access the site.

Based on the above findings and conditions, the Zoning Ordinance requirements in Section 3.4.6.F 3 pertaining to submittal of a traffic study with this special exception application are hereby waived.

If you have any questions about the contents of this letter please contact me at 703-771-2770 or via email at sberryhill@leesburgva.gov.

Sincerely,



Susan Berry Hill, AICP
Director Planning and Zoning
Town of Leesburg

Attachment: Memo from Michael Armstrong, Department of Public Works

cc: Mike Watkins, Senior Planner, DPZ
Michael Armstrong, Engineer, DPW

- 3.(10) (Plat Content) Label all lines on all sheets. For example, provide a label for the 100-yr. overland relief path on Sheet 1. Also label all existing easements and provide recordation references. (Sht. 1-5) Z.O. 3.4.6.E.20. *Existing contours are not shown on Sheet 4 and the proposed contours are not labeled. Provide this information with the next submission. (Sht. 4) Z.O. 3.4.6.E.20.*

Storm Drainage:

- 4.(12) (BMP Requirements) With regard to Note 17, provide some details of a proposed strategy and conceptual layout including preliminary calculations for meeting the requirements of a BMP "Hot Spot". (Sht. 1) DCSM 10-120.2G(8). *At a minimum, provide the following narratives and notes regarding "Hot Spot" compliance on the Special Exception Plat:*

BMP Hot Spot Narrative

This site will operate as a fuel storage and dispensing station and is considered a Hot Spot per DCSM (5-640.4J). A Baysaver or equivalent approved product will be required to contain minor spills on site. The designed system will provide a primary separation manhole, secondary storage manhole (or facility) and Baysaver separator (or approved equivalent). A shutoff valve, easily accessible and signed, is to be installed on the downstream side of Baysaver or equivalent. In the event of spill, the valve is to be closed and the spill contained within the pipes and structures which will be over-sized to hold the contaminated rainwater and material until HAZMAT teams reach the site. Secondary storage and/or storage facility shall be designed to contain a minimum of 2500 gallons.

Notes:

1. BMP Hot Spot structure details to be provided on Final Site Plan.
 2. BMP Hot Spot Maintenance Plan to be provided on Final Site Plan.
 3. BMP Hot Spot site specific Narrative to be provided on Final Site Plan.
 4. BMP Hot Spot structure sizing computations to be provided on Final Site Plan.
 5. BMP Hot Spot structure capacities to be provided on Final Site Plan.
 6. The Applicant shall provide evidence that an approved Emergency Response Plan has been filed with and approved by the Town as well as the Loudoun County Department of Fire and Rescue Services.
- 5.(13) (Storm Sewer Configuration) The storm sewer layout shown does not seem to take into consideration the future development of adjoining lots and does not provide curb inlets at intersections and entrances. Revise the storm sewer layout to include the location of curb inlets, provide 90° crossings of roads, utilities and access ways and minimizing the number of manhole structures in the paved areas of the access ways, while still providing points of connection for BMP facilities and future development in order to demonstrate that this can be accomplished without modifications. (Sht. 5) Z.O. 3.4.6.E.23. *The storm sewer layout should be revised (simplified) further by extending the trunk line to the inlet closest to Miller Drive before crossing the street, thus providing a usable landscaped area just south of the proposed "air and vac" island. (Sht. 2-5) Z.O. 3.4.6.E.23.*

Transportation/Vehicular Access:

- 6.(22) (Traffic Circulation) The right/left turn lanes at the western side of Trimble Way does not appear to function properly in the interim (where the proposed gas station facility is the only structure located on this landbay) or ultimate conditions, however, based upon the proximity of the site entrance (being within 200') to the public right of way, a complete, updated traffic study will be required prior to any future development in Land Bay C. *There are similarities between the proposed major access point for this gas station site (proposed adjacent to Miller Drive) and the existing gas station (Sheetz, located adjacent to Edwards Ferry Road). In*

both of these examples, a two way private travelway, with a single lane in each direction and a very minimal left turn lane into the site, serves or will serve the respective gas stations. In an effort to avoid another "Sheets" condition with this project (i.e. excessive stacking in the public right of way, it is recommended that the Applicant provide two lanes into the site from Miller Drive (onto Trimble Plaza) with the innermost lane dropping off as a left turn only lane into the proposed gas station. The outer lane would then enhance internal vehicular circulation by serving as a bypass lane for access to the rest of the Landbay which would minimize turning movement conflicts that potentially could cause excessive stacking in the Miller Drive public right of way. Z.O. 3.4.12.D.

- 7.(27) (Roadway Network Configuration) The tanker truck turning movement analysis appears to have some conflicts. At the entrance into the site from Trimble Plaza, the truck swings into the left lane in order to make a right turn and appears to clip the curb on the right hand side. Also the turn from access way at station onto Trimble Plaza appears to clip the curb. Review and revise to provide for safe maneuvering of the design vehicle. (Sht. 2) Z.O. 3.4.6.E.7. *The tanker truck turning movement analysis appears to still have some conflicts. When the truck exits Trimble Plaza onto Battlefield Parkway, the truck swings into the left lane in order to make a right turn and creates an unsafe condition. Recommend revising access road around the gas station itself, to accommodate WB-50 vehicles if there are cars waiting to turn at intersections, it does not create an unsafe condition. Review and revise to provide for safe maneuvering of the design vehicle. (Sht. 2) Z.O. 3.4.6.E.7.*

Landscaping:

- 8.(31) (Screening between Uses) Sheet 1, Note 13, fourth sentence states: "The other boundaries are part of commercial Land Bay C under the ZM#159 concept plan in which all uses are intended to interrelate with one another without a need to be screened and buffered from each other, and since the screening and buffering is being provided around the perimeter of the land bay adjacent to public roads under the proffered landscape plan for ZM#159".

The Rezoning Concept Plan/proffers did not include this language.

Revise the statement (note) regarding perimeter parking lot screening in the upper left hand corner of Sheet 4 to read: "The perimeter parking lot screening adjacent to public streets is not required as an S-2 or S-3 screen has been provided adjacent to the public streets around the entire perimeter of Land Bay C per the Oaklawn at Stratford Rezoning ZM-159 and the Oaklawn Commercial Preliminary Subdivision Plat TLPS-2004-0003." Z.O. 12.5.1.

Provide a continuous landscape area at least five (5) feet in width between the parking lot and the property line. See attachment showing areas where perimeter parking lot screening is required. (Sheet 4) Z.O. 12.5.4.A.

George Clausen, LSIT
Senior DPR Engineer

John D. Johnston, LA
Senior DPR Planner



The Town of Leesburg
DEPARTMENT OF PLANNING AND ZONING

TO: Irish Grandfield
Project Manager

DATE: November 19, 2012

FROM: Kevin Lawlor
Sr. Planner – Zoning

RE: TLSE-2012-0005
2nd Submission

I have reviewed the proposed Oaklawn Landbay C gas station/convenience store/carwash, engineer date 10/12/12, received date 10/15/12, prepared by Paciulli Simmons & Associates. The applicant proposes to build a gas station/convenience store/carwash (4365 s.f.) in Landbay C.

I provide the following information in review of the request.

1. B.A.R. approval is required for this project in accordance with the proffers from TLZM-1999-0159.

Kevin P. Lawlor



The Town of Leesburg
INTEROFFICE MEMORANDUM
DEPARTMENT OF PUBLIC WORKS

TO: Irish Grandfield, Environmental Planner
FROM: Michael P. Armstrong, EIT, LSIT, Engineer
DATE: November 19, 2012
RE: Oaklawn – Gas Station, Convenience Store, and Car Wash

The following comments are offered by the Department of Public Works Transportation Division to be incorporated into the Department of Planning & Zoning comments to be addressed by the applicant. Please advise the applicant to revise the plans in accordance with the comments below and resubmit.

1. Due to the anticipated 95th percentile queue, it is recommended that a “Do Not Block Intersection” sign be installed for northbound traffic on Trimble Way prior to the main entrance to the gas station.

Michael P. Armstrong, EIT, LSIT
Engineer

CC: Calvin K. Grow, P.E., Transportation Engineer



The Town of Leesburg
INTEROFFICE MEMORANDUM
DEPARTMENT OF UTILITIES

To: James P. ("Irish") Grandfield, AICP, Senior Planner

From: *AE* Aref Etemadi, Deputy Director of Utilities

Date: November 1, 2012

Subject: Oaklawn Landbay C, Gas Station
TLSE – 2012-0005
2nd Submission

We offer the following comments:

1. Due to the angle of sewer penetration at manhole "a" the better solution will be to make this connection at manhole "S22" (previous comment#5) unless the lateral enters the manhole at bench.
2. Show the water service to the store (previous comment#6).
3. The angle of the crossing of 66" storm and the waterlines is unacceptable unless the water is above the storm (previous comment#7).
4. If food preparation is proposed within the store a grease trap or interceptor will be required and must be shown or added as a note to the plans (previous comment#8).
5. Is a lawn irrigation system proposed for this use?
6. Cleanouts are required 5' from the front of the buildings on both lateral sewers exiting the car wash and the store.
7. See attached requests for changes to utility layout.
8. Please respond to previous comments 9-12.

Oaklawn, Landbay C
Special Exception Application for an
Automobile Service Station, Convenience Store and Car Wash
Written Statement
April 3, 2012
Revised October 12, 2012

I. Introduction

Oaklawn Development Partners, LLC is filing a special exception application for an automobile service station, convenience store and car wash on 1.15 acre tract of land within Land Bay C of the Oaklawn Planned Employment Community (PEC) development. The 1.15 acres currently is part of 3 separate parcels that will be consolidated into one parcel subsequent to the approval of the proposed special exception application. The Oaklawn PEC district is subject to the proffers for ZM #159 and TLZM 2005-0002. The Oaklawn PEC has been planned for two service stations within the community; however, the proffers for TLZM 2005-0002 stipulated that a special exception would be required if one the of the service stations were to locate in Land Bay C.

II. Type of Operation

A service station is proposed with eight fueling stations containing 16 gas pumps. Along with the service station will be a 3,525 square foot convenience food store and an 825 square foot car wash.

III. Hours of Operation

This facility will operate 24 hours a day, seven days a week.

IV. Traffic Impacts

The traffic studies submitted for ZM #159 and TLZM 2005-0002 analyzed the proposed special exception use. The most recent traffic study analyzed a gas station in Land Bay C as one of the two gas stations within the Oaklawn development. Oaklawn has proffered and phased road improvements based on this study. The impacts of the proposed special exception uses were accounted for in these prior studies and in the proffered and phased road improvements.

V. Impacts on adjacent uses and measures proposed to mitigate such impacts.

The governing Oaklawn PEC zoning with concept plan and proffers created a comprehensively planned community that accounted for a variety of uses needed to create a thriving employment center. Two gas stations always have been contemplated within Oaklawn. In Land Bay C, the proposed special exception site is surrounded by future uses that could include office, light industrial, data center, free-standing restaurant, fast food with drive through and bank with drive-through. One of the Mixed-Use Center land bays is located across Miller Drive from the proposed special exception uses, which is planned for similar uses proposed in this application, as well as office and residential uses. Oaklawn has been planned with a unified landscaping plan along the public streets, and the special exception uses will comply

with the unified landscaping plan along Miller Drive. The uses internal to Land Bay C are intended to be integrated with one another using a coordinated private driveway network.

VI. Compliance with Section 3.4.12 Approval Criteria.

A. The proposed use will not adversely affect the use of neighboring properties.

An amendment to TLZM 2005-0002 in 2007 relocated the 2nd gas station from MUC 2 to Land Bay C for the purpose of moving it further from the residential units in Oaklawn. This proposed Special Exception fulfills the commitment of that proffer amendment. As for adjacent uses, the gas use is fully compatible with the other planned uses such as office, retail, bank, restaurant, pharmacy and personal service.

B. The use shall comply with applicable zoning district regulations and applicable provisions of the adopted *Town Plan*, including but not limited to the Plan's Land Use Compatibility policies.

Land Bay C within Oaklawn is designated for regional office use on the Planned Land Use Map of the Town Plan. Oaklawn is a planned employment community for which the concept plan accommodated locations for the primary uses, as well as the supporting uses. The concept plan for Land Bay C, in particular, designated restaurant, bank and service station uses along with the office uses as part of this regional office community. The regional office designation contemplates the need for retail and service uses for the daily needs of the workers, customers and businesses in the development. This special exception use proposes to provide such uses as contemplated in the concept plan for Oaklawn. Development within Oaklawn is subject to the H-2 Corridor Design Guidelines to ensure architectural compatibility among the various uses within Oaklawn.

C. The location, size and height of buildings, structures, walls and fences, and the nature and extent of screening, buffering and landscaping shall be such that the use will not hinder or discourage the appropriate development and use of adjacent or nearby land and/or buildings.

The proposed uses have been planned as part of the other Land Bay C uses, which include an office building, restaurant and pharmacy uses surrounding the proposed uses. The internal private street network accommodates these uses, and none of the uses interfere with each other's operations. The proposed buildings will be one-story, and the service station canopy will comply with the Zoning Ordinance maximum height requirement. Oaklawn has a unified landscape plan bordering all of the public streets within Oaklawn. The site containing the proposed uses fronts on Miller Drive, and the Miller Drive landscaping will be provided in accordance with the required buffer and the Oaklawn landscape plan.

A six-foot wide trail also will be constructed within the setback along Miller Drive as part of the Oaklawn trail system.

D. The proposed use will be such that pedestrian and vehicular traffic generated will not be hazardous or conflict with the existing and anticipated traffic in the neighborhood and on the streets serving the site.

Both the vehicular and pedestrian access is part of the networks planned for the Oaklawn planned development. The proposed special exception uses have been contemplated as part of the Oaklawn planned development from its inception; therefore, the traffic generated by these uses was anticipated in the initial planning for Oaklawn.

VII. Compliance with Section 9.3, Use Standards

A. Section 9.3.3 Car Washes

A. Car washes shall be located and designed so that vehicular circulation shall not conflict with traffic movements in adjacent streets, service drives, driveways and or parking areas.

The car wash entrance is accessed from an internal driveway that serves this use. The vehicles exiting the car wash will use the site entrance that accesses the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Car washes (as an accessory use) shall have no separate and exclusive curb cut access to the abutting roadway.

The proposed accessory car wash use has an access point from a driveway and vehicles exiting the car wash will use the site's entrance onto the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Section 9.3.6 Convenience Food Store

A. No convenience food store shall be located on a lot adjacent to residential dwelling units.

There are no dwelling units located on the lots adjacent to the proposed special site.

C. All business services, storage and display of goods shall be conducted within a completely enclosed building.

The applicant shall adhere to this requirement once the convenience food store is operating.

D. Section 9.3.21 Service Stations

- A.** *Service stations shall not include ancillary uses such as vehicular or tool rental (including moving vans) and shall be limited to the servicing of vehicles and nonautomotive accessory retail sales of snacks, convenience foods, and similar products.*

Vehicle servicing is not proposed with this application nor is vehicle rentals. Retail sales will occur as part of the convenience food store use.

- B.** *Service stations shall not include an outdoor storage area for more than three (3) abandoned, wrecked or inoperable vehicles on the site for more than one week, subject to the limitation that there shall be no dismantling, wrecking, or sale of said vehicles or part(s) thereof.*

There will be no inoperable vehicles, since servicing of vehicles is not proposed with this service station.

- C.** *Outside sales and display areas shall be shown on the plan to be reviewed by the Planning Commission and Town Council.*

Outside sales and display are not proposed with this use.

- D.** *The parking of commercial vehicles shall be prohibited on the site of a service station. This provision shall not be interpreted as prohibiting the parking of vehicles actively engaged in delivering fuel or other supplies to the service station.*

The applicant will comply with this requirement.

- E.** *No permit shall be issued for a service station if the entrance or exit for vehicles is*

- 1.** *In the same block front within 200 feet of any school, public playground, hospital, church, or public library; or*

None of these uses are included in Oaklawn.

- 2.** *If such entrance or exit is be located within 20 feet of an “R” District within the same block front.*

The entrance for the service station is internal to Land Bay C, which does not contain any R District zoning.

- 3.** *No permit shall be issued for a service station if any part of any structure, including underground gasoline tanks or service aisles, is located within 100 feet of any*

building or grounds of a school, public playground, hospital, church, or public library or any “R” District.

None of these uses exist within Oaklawn.

- F.** *Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14’0”. The overall height of canopies shall not exceed 17’3”.*

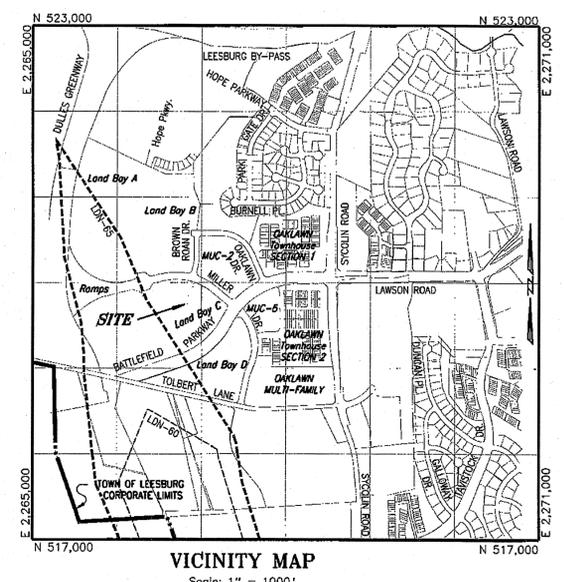
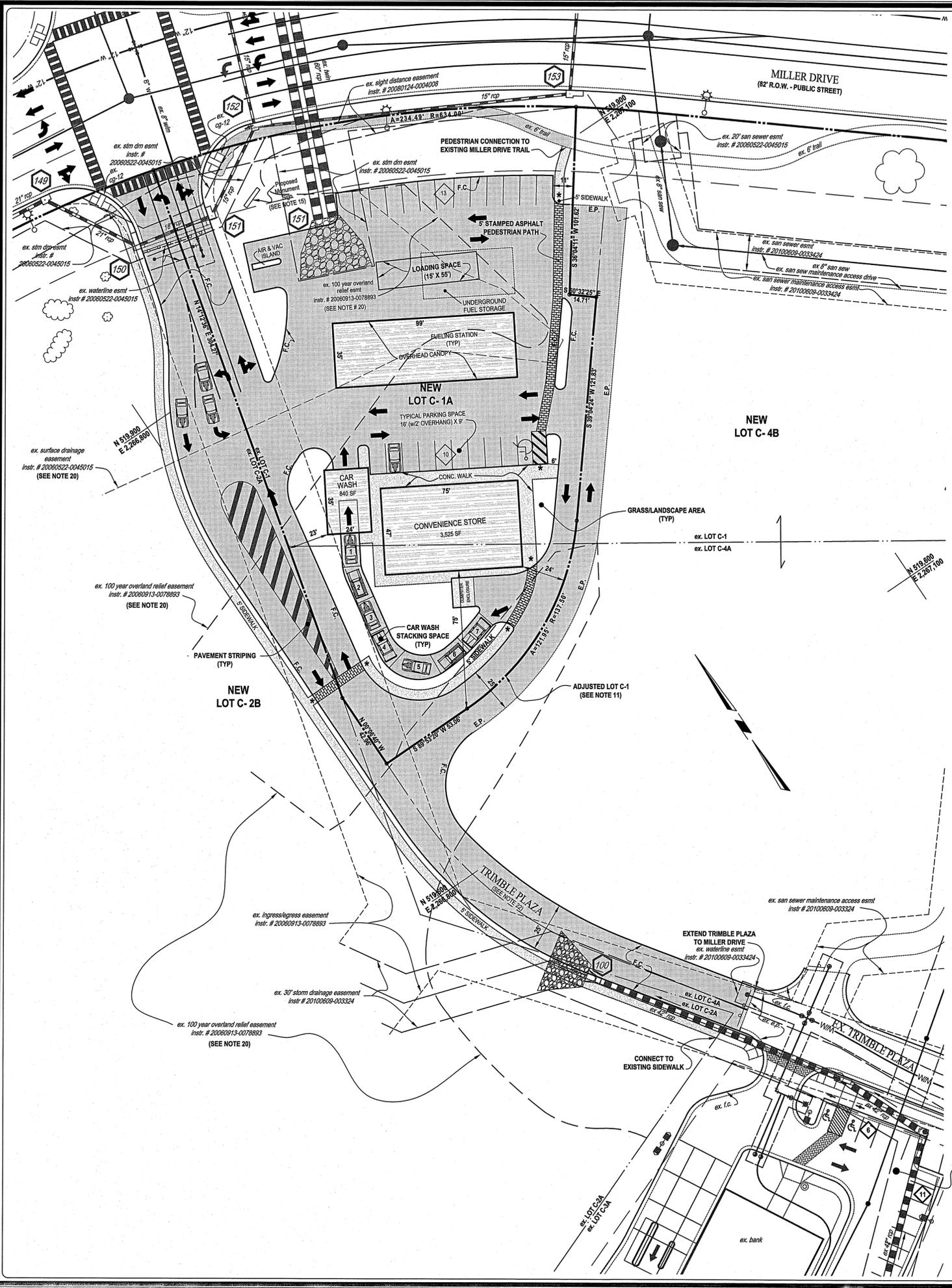
A modification request pursuant to Section 3.4.3 of the Zoning Ordinance has been submitted to modify the canopy height fascia not to exceed 17’0" and the overall height not to exceed 20’6".

- G.** *Canopy lighting shall be fully recessed in the ceiling of the canopy.*

The proposed canopy lighting will comply with this requirement.

- H.** *All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, color and detailing.*

Oaklawn is subject to the H-2 Corridor Design Guidelines and process. Those guidelines and process can ensure that this provision will be adhered to by the proposed uses.



PARKING TABULATION

REQUIRED: CAR WASH (SELF-SERVE) - [1 SPACE/EMPLOYEE x 0 EMPLOYEES] = 0 SPACES
 CONVENIENCE STORE - [6 SPACES/1000 SQ.FT. x 3,525 SQ.FT.] = 21 SPACES

PROVIDED: 23 SPACES (INCLUDES 1 ACCESSIBLE SPACE)

LOADING SPACE: 1 SEMI-TRAILER LOADING SPACE IS REQUIRED AND PROVIDED

CAR WASH STACKING SPACES: 7 SPACES REQUIRED AND PROVIDED (18' x 10' MINIMUM)

ZONING REQUIREMENTS (PER ZM-159)

MINIMUM LOT WIDTH:	100'	PROVIDED 200'
MINIMUM LOT AREA:	20,000 SQ.FT.	57,064 SQ.FT.
MAXIMUM BUILDING HEIGHT:	80'	25'
MINIMUM SETBACKS:		
FRONT:	35'	106'
SIDE:	15'	23'
REAR:	15'	75'
MAXIMUM FLOOR AREA RATIO:	0.60 (INDIVIDUAL LOT) 0.40 (PER DISTRICT AS A WHOLE)	0.09

AREA TABULATION

AREA OF ADJUSTED LOT C-1 (AND TOTAL AREA AFFECTED BY THIS SPECIAL EXCEPTION; SEE NOTE 11.) 1.31 AC. (57,064 SQ.FT.)

PROPOSED F.A.R. = 4,365/56,746 = 0.08

CANOPY HEIGHT

MODIFICATION REQUESTED (in accordance with zoning ordinance section 3.4.13) to zoning ordinance section 9.3.24.F as follows:

Canopy Height as measured from the average finished grade to the lowest point on the canopy fascia should not exceed 17'0". The overall height of canopy shall not exceed 20'6".

AREA TABULATION

OLD		NEW	
LOT	AREA	LOT	AREA
C-1	2.57 AC.	C-1A	1.31 AC.
C-2A	5.17 AC.	C-2B	5.17 AC.
C-4A	2.55 AC.	C-4B	3.81 AC.
TOTAL	10.29 AC.	TOTAL	10.29 AC.

LOT C-3A INCLUDES AN EXISTING BANK W/ DRIVE THRU AND RELATED SITE IMPROVEMENTS

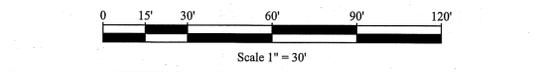
- NOTES:**
- ZONING DISTRICTS: PEC AND AI (PER ZM-159 & TLZM-2005-0002).
 - PROPOSED USE: AUTOMOBILE SERVICE STATION w/ CONVENIENCE STORE AND CAR WASH
 - PROPERTY IDENTIFICATION: THIS SPECIAL EXCEPTION IS LOCATED WITHIN PORTIONS OF THE FOLLOWING:

PIN #	LOT #	Current Area
233-20-0550	(OAKLAWN COMMERCIAL - LOT C-1)	2,5678 Ac.
233-29-6350	(OAKLAWN COMMERCIAL - LOT C-2A)	5,1729 Ac.
233-29-9822	(OAKLAWN COMMERCIAL - LOT C-4A)	2,5476 Ac.
		TOTAL: 10,2883 Ac.
 - ↻ DENOTES TRAFFIC LANES AND/OR DIRECTION OF TRAVEL.
 - THIS SPECIAL EXCEPTION APPLIES TO PORTION OF LANDBAY C DESIGNATED HEREON ONLY.
 - SITE LIGHTING SHALL COMPLY WITH TOWN OF LEESBURG D.C.S.M. AND ZONING ORDINANCE REQUIREMENTS.
 - PERIMETER PARKING LOT LANDSCAPING SHALL COMPLY WITH ZONING ORDINANCE SECTION 12.5. (SEE SHEET 4)
 - INTERIOR PARKING LOT LANDSCAPING SHALL COMPLY WITH ZONING ORDINANCE SECTION 12.6. (SEE SHEET 4)
 - PEDESTRIAN CIRCULATION SYSTEM SHALL COMPLY WITH ZONING ORDINANCE SECTION 11.2.3.B WHERE CROSSING TRAVELWAYS.
 - PEDESTRIAN CIRCULATION SYSTEM SHALL BE LIGHTED IN ACCORDANCE WITH ZONING ORDINANCE SECTION 11.2.4.
 - THE RECONFIGURATION OF PROPOSED LOTS C-1, C-2A AND C-4A SHALL BE APPROVED AND RECORDED PRIOR TO SITE PLAN APPROVAL.
 - DEVELOPMENT IS SUBJECT TO ZONING ORDINANCE USE REGULATIONS 9.3.3 (CAR WASH), 9.3.8 (CONVENIENCE FOOD STORE) AND 9.3.24 (SERVICE STATIONS).
 - NOTE DELETED
 - SPEAKERS WILL BE LOCATED AT EACH FUELING STATION, AND SHALL COMPLY WITH ZONING ORDINANCE SECTION 7.9.
 - SIGNAGE SHALL COMPLY WITH ZONING ORDINANCE ARTICLE 15. PROPOSED MONUMENT SIGN LOCATION SUBJECT TO SIGN PERMIT APPROVAL.
 - A GREASE-OIL-GRIT-WATER SEPARATOR WILL BE REQUIRED FOR THE PROPOSED CAR WASH. DETAILS SHALL BE PROVIDED WITH THE FINAL SITE PLAN SUBMISSION.
 - STORMWATER RUNOFF QUALITY CONTROL: THIS SITE IS SERVED BY THE STRATFORD LAKE BMP FACILITY. ADDITIONALLY, THIS SITE IS SUBJECT TO THE "HOT SPOT" REQUIREMENTS OF DCSM 5-640 (4) (J) (1) FOR FACILITIES ENGAGED IN STORING OR DISPENSING PETROLEUM PRODUCTS, AND DCSM 5-640 (5), (SWPPP). SEE SHEETS 7 AND 8.
 - APPROVAL OF THE SPECIAL EXCEPTION DOES NOT EXPRESS OR IMPLY ANY WAIVER OR MODIFICATION OF THE REQUIREMENTS SET FORTH IN THE SUBDIVISION AND LAND DEVELOPMENT REGULATIONS, THE ZONING ORDINANCE, OR THE DESIGN AND CONSTRUCTION STANDARDS MANUAL (DCSM) EXCEPT AS SPECIFICALLY APPROVED BY TOWN COUNCIL OR AS SHOWN AS APPROVED BY TOWN COUNCIL ON THE SPECIAL EXCEPTION PLAT AND SUPPORTING DOCUMENTS.
 - DEVELOPMENT OF THIS SPECIAL EXCEPTION USE SHALL BE IN SUBSTANTIAL CONFORMANCE WITH THIS SPECIAL EXCEPTION PLAT WITH REASONABLE ALLOWANCES TO BE MADE FOR ENGINEERING AND DESIGN ALTERATIONS TO MEET TOWN SUBDIVISION AND LAND DEVELOPMENT REGULATIONS, ZONING ORDINANCE OR DESIGN AND CONSTRUCTION STANDARDS MANUAL REQUIREMENTS.
 - EASEMENT FOR OVERLAND RELIEF AND SURFACE DRAINAGE TO BE MODIFIED WITH THE FINAL SITE PLAN SUBMISSION, IN ACCORDANCE WITH PROPOSED GRADING AND SUPPORTING ENGINEERING CALCULATIONS.
 - * DENOTES LOCATION OF ACCESSIBLE CURB RAMP.
 - TRIMBLE PLAZA IS A VARIABLE WIDTH PRIVATELY MAINTAINED PARKING LOT TRAVEL AISLE.

SHEET INDEX

1.	SPECIAL EXCEPTION PLAT FOR AUTOMOBILE SERVICE STATION w/ CONVENIENCE STORE AND CAR WASH	
2.	LOADING SPACE AUTOTURN ANALYSIS	
3.	DUMPSTER AUTOTURN ANALYSIS	
4.	LANDSCAPE EXHIBIT	
5.	CONCEPTUAL GRADING PLAN	
6.	X-SECTIONS	
7.	CONCEPTUAL BMP PLAN	
8.	CONCEPTUAL STRATFORD LAKE BMP COMPUTATIONS	

3	ADDRESS 2ND TOWN REVIEW COMMENTS	12/31/2012
2	ADDRESS 1ST TOWN REVIEW COMMENTS	10/12/2012
1	ADDRESS INITIAL TOWN COMMENTS FOR ACCEPTANCE	5/25/2012
NO.	SHEET NUMBER AND REVISION DESCRIPTION	DATE



PACIULLI

SIMMONS & ASSOCIATES
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Planners
Surveyors
Landscape Architects
Wetland Specialists
Environmental Scientists
Archaeologists

**OAKLAWN - COMMERCIAL
LANDBAY C**

TOWN OF LEESBURG
LOUDOUN COUNTY, VIRGINIA

TLSE-2012-0005

SPECIAL EXCEPTION PLAT
FOR AUTOMOBILE SERVICE STATION w/
CONVENIENCE STORE AND CAR WASH

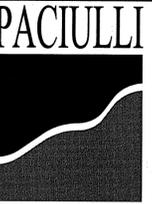
PROFESSIONAL ENGINEER

Samuel Jackson Williams, III
Lic. No. 017783

12-31-12

DATE: 04-05-2012
FILE NO: L190.012.B
DRN: REKSR
CKD: S.J.W.III

SHEET 1 of 8

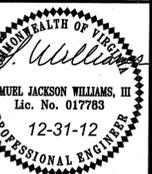


SIMMONS & ASSOCIATES
Established 1744

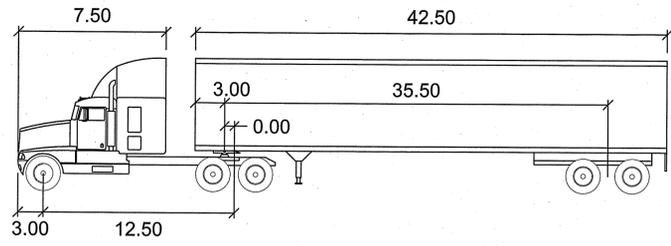
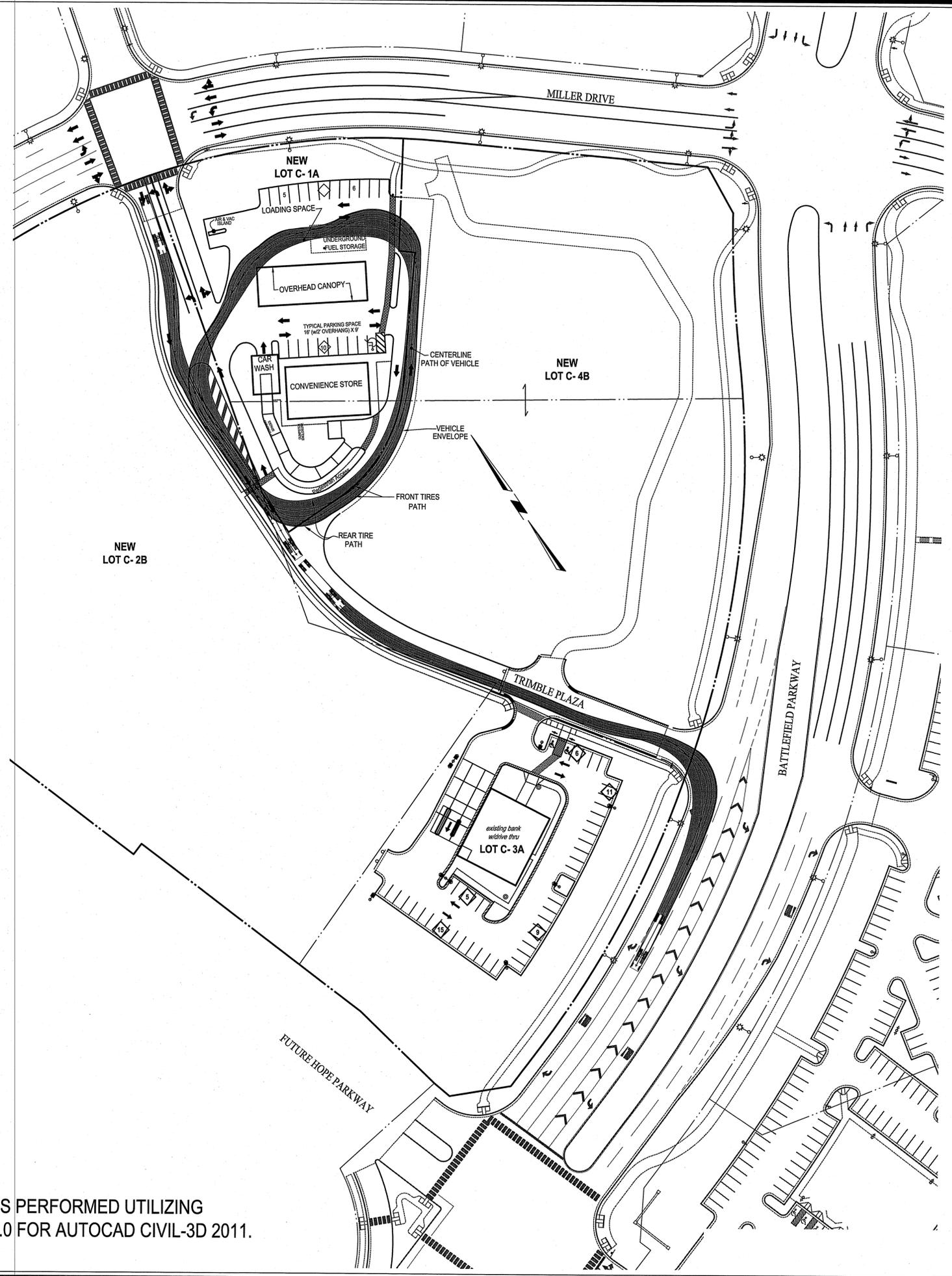
50 Catocin Circle, NE
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**OAKLAWN - COMMERCIAL
LANDBAY C**
TOWN OF LEESBURG
LOUDOUN COUNTY, VIRGINIA

**LOADING SPACE AUTOTURN
ANALYSIS**



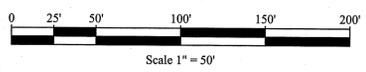
DATE: 04-05-2012
FILE NO: L190.012.B
DRN: REKSR
CKD: S.J.V.III



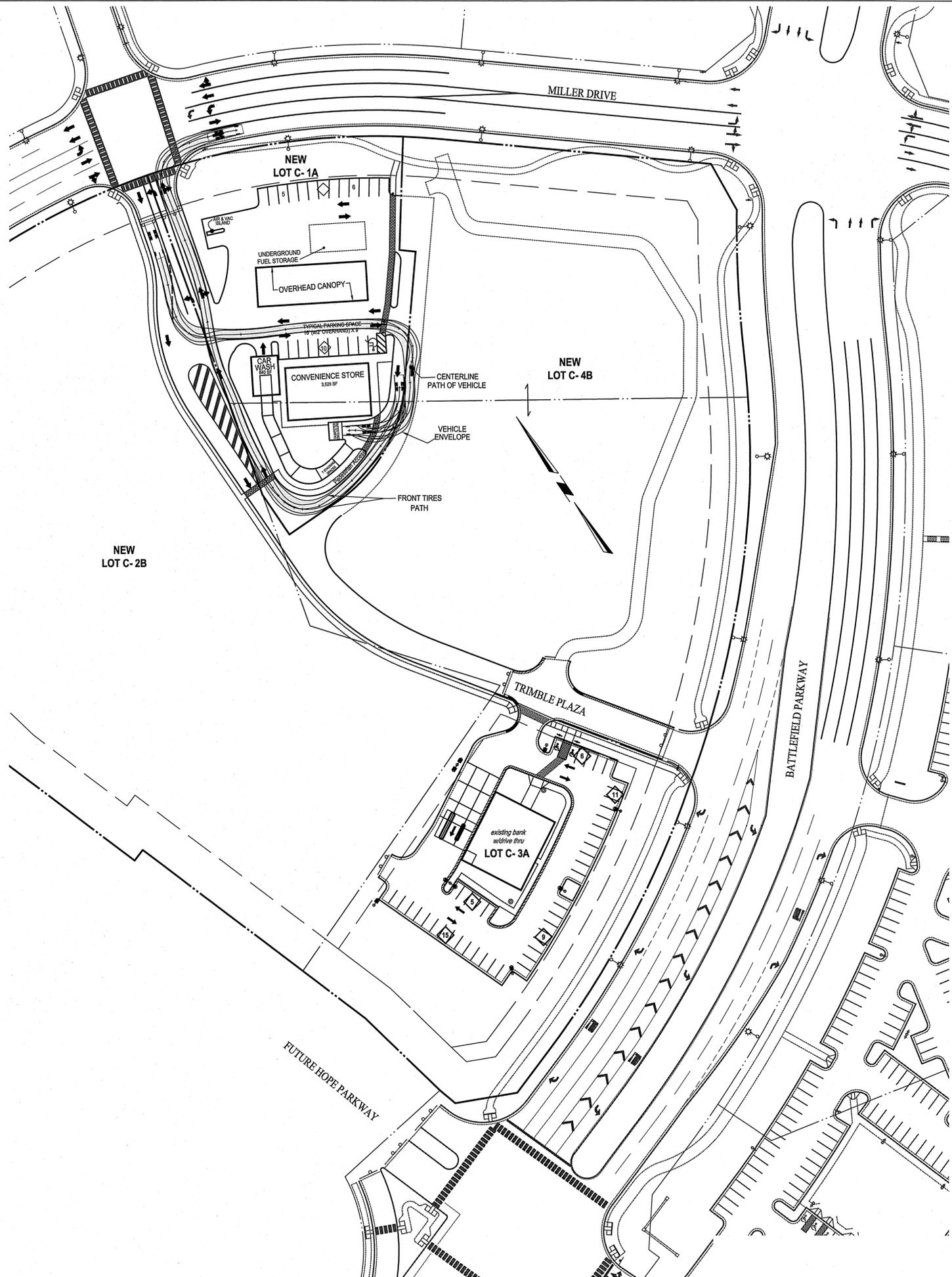
**AASHTO WB-50 TRUCK DETAIL
UTILIZED IN THIS ANALYSIS**
N.T.S.

NOTE:
THE ANALYSIS PERFORMED UTILIZING
AUTOTURN 7.0 FOR AUTOCAD CIVIL-3D 2011.

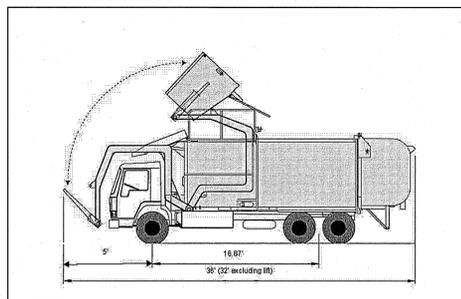
**THIS SHEET FOR PURPOSE OF
DEMONSTRATING ADEQUATE
TRUCK MANEUVERABILITY ONLY!!!**



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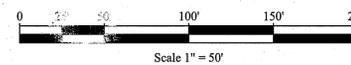


TRUCK DETAIL UTILIZED IN THIS ANALYSIS
GARBAGE TRUCK
N.T.S.



NOTE: THIS ANALYSIS PERFORMED UTILIZING AutoTURN 7 for Autodesk Civil 3D 2010

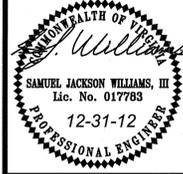
THIS SHEET FOR PURPOSE OF
DEMONSTRATING ADEQUATE
TRUCK MANEUVERABILITY ONLY!!!



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**OAKLAWN - COMMERCIAL
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**DUMPSTER AUTOTURN
ANALYSIS**



DATE: 04-05-2012
FILE NO: L190.012.B
DRN: REKSR
CKD: S.J.W.III

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PERIMETER PARKING LOT TABULATION

INDICATES LOCATION OF REQUIRED 5' WIDTH PERIMETER PARKING LOT LANDSCAPING (NOT ADJACENT TO PUBLIC STREET)

NOTE: THE PERIMETER PARKING LOT SCREENING TO PUBLIC STREETS IS NOT REQUIRED AS AN S-2 OR S-3 SCREEN HAS BEEN PROVIDED ADJACENT TO THE PUBLIC STREETS AROUND THE ENTIRE PERIMETER OF LAND BAY C PER THE OAKLAWN AT STRATFORD REZONING ZM-159 AND THE OAKLAWN COMMERCIAL SUBDIVISION PLAT TLPS-2004-0003.

INTERIOR PARKING LOT TABULATION

INDICATES AREA OF INTERIOR PARKING LOT LANDSCAPING PER Z.O. SECTION 12.6.

GROSS PARKING LOT AREA = 31,372 SF.
 MINIMUM REQUIRED: 5% x 31,372 SF. = 1,569 SF.
 PROVIDED = 3,305 SF.

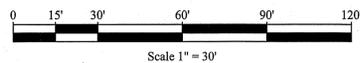
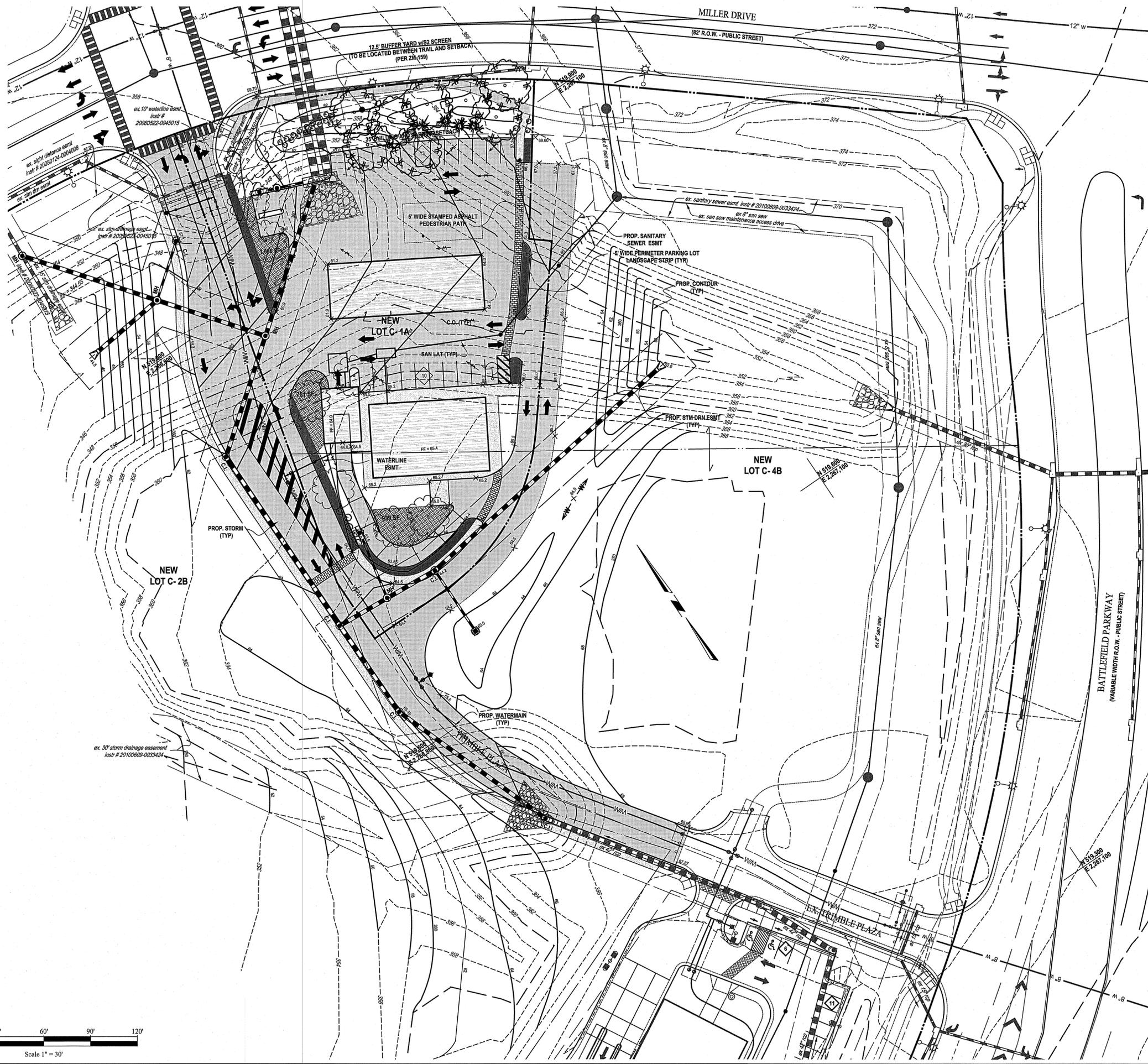
LEGEND

-  EVERGREEN TREE
-  LARGE CANOPY TREE
-  MEDIUM CANOPY TREE
-  UNDERSTORY TREE
-  SHRUB

NOTE:

PURPOSE OF THIS SHEET IS TO DEMONSTRATE CAPABILITY OF THIS LAYOUT TO COMPLY WITH ZONING ORDINANCE LANDSCAPE REQUIREMENTS. FINAL LANDSCAPE DESIGN INCLUDING LOCATION, SIZE, AND SPECIES SUBJECT TO FINAL SITE PLAN APPROVAL.

BUFFER YARD WAIVER REQUESTED FOR PORTION OF SITE PERIMETER NOT ADJACENT TO MILLER DRIVE (SEE NOTE 13).



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 LANDBAY C**

TOWN OF LEESBURG
 LOUDOUN COUNTY, VIRGINIA

LANDSCAPE EXHIBIT

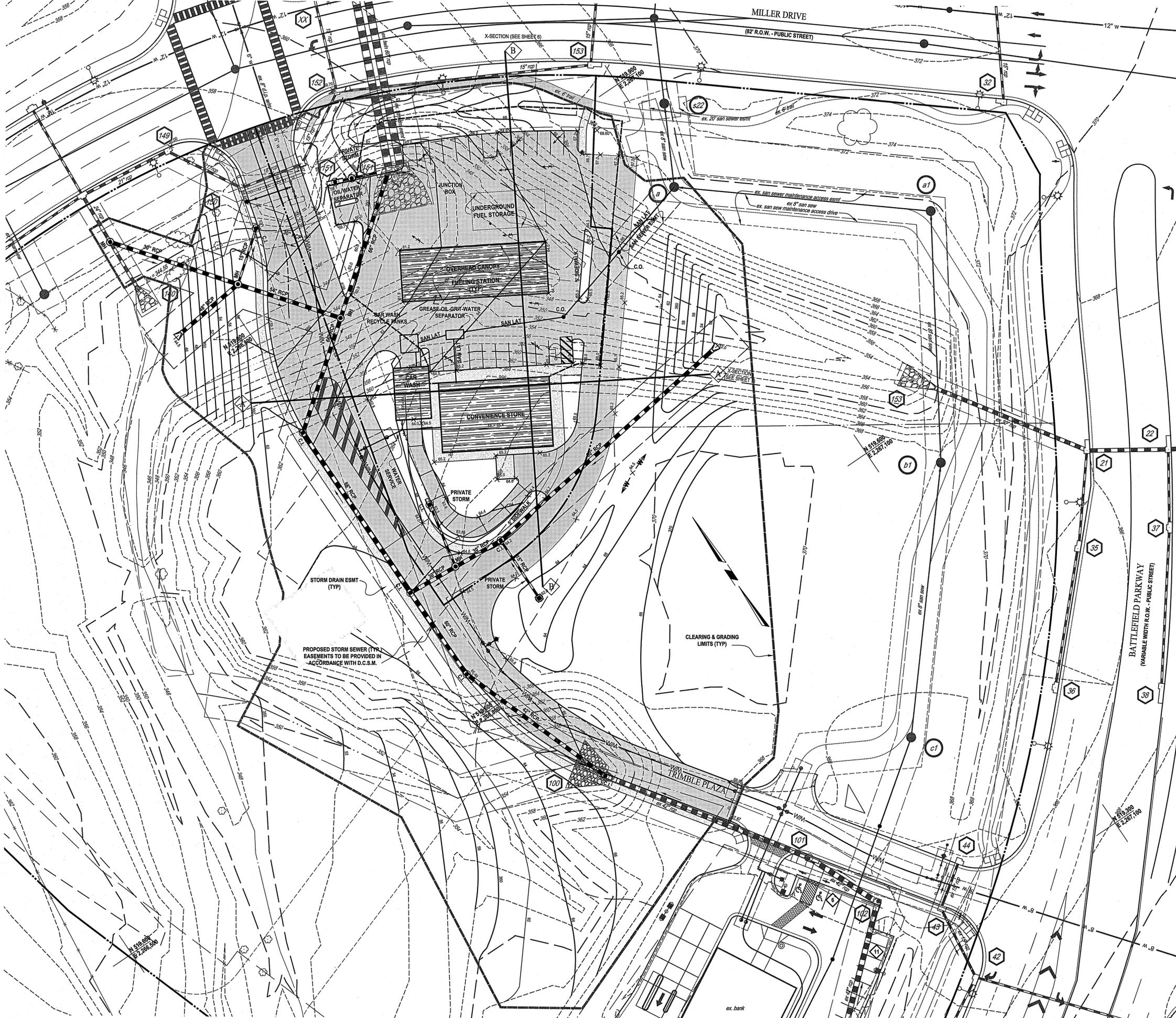
STATE OF VIRGINIA

Samuel Jackson Williams, III

SAMUEL JACKSON WILLIAMS, III
 Lic. No. 017783
 12-31-12
 PROFESSIONAL ENGINEER

DATE: 04-05-2012
 FILE NO: L190.012.B
 DRN: REKSR
 CKD: S.J.W.III

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NOTES:
 GRADING SHOWN IS CONCEPTUAL, AND IS INTENDED TO SHOW GENERAL FINISH GRADE RELATIONSHIP AND SURFACE DRAINAGE FLOW DIRECTIONS. NOT FOR CONSTRUCTION.

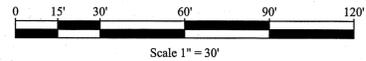
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**OAKLAWN - COMMERCIAL
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 LOUDOUN COUNTY, VIRGINIA

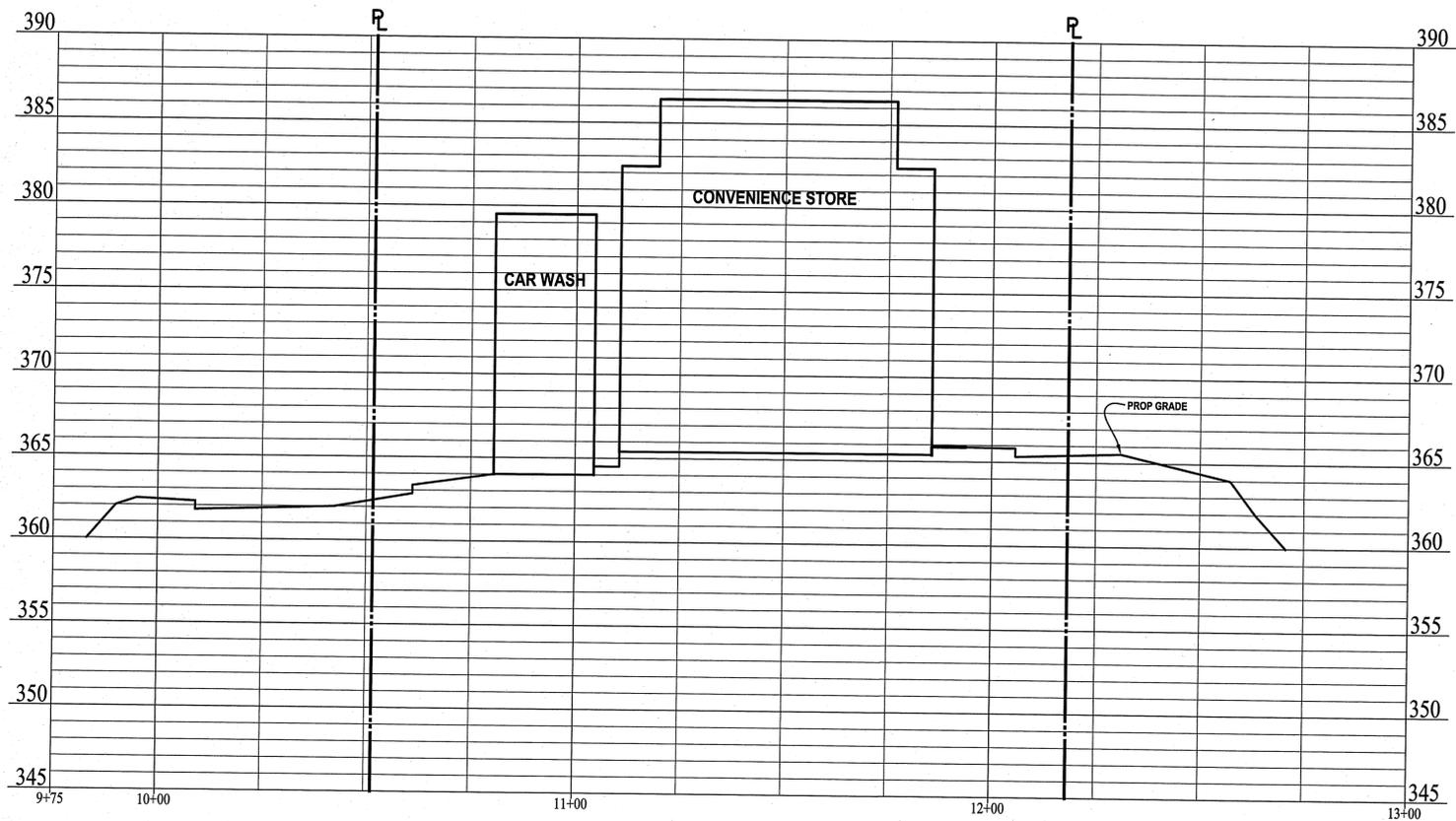
**CONCEPTUAL GRADING PLAN
 AND UTILITY LAYOUT**

COMMONWEALTH OF VIRGINIA
Samuel Jackson Williams, III
 SAMUEL JACKSON WILLIAMS, III
 Lic. No. 017783
 12-31-12
 PROFESSIONAL ENGINEER

DATE: 04-05-2012
 FILE NO: L190.012.B
 DRN: REKSR
 CKD: S.J.W.III

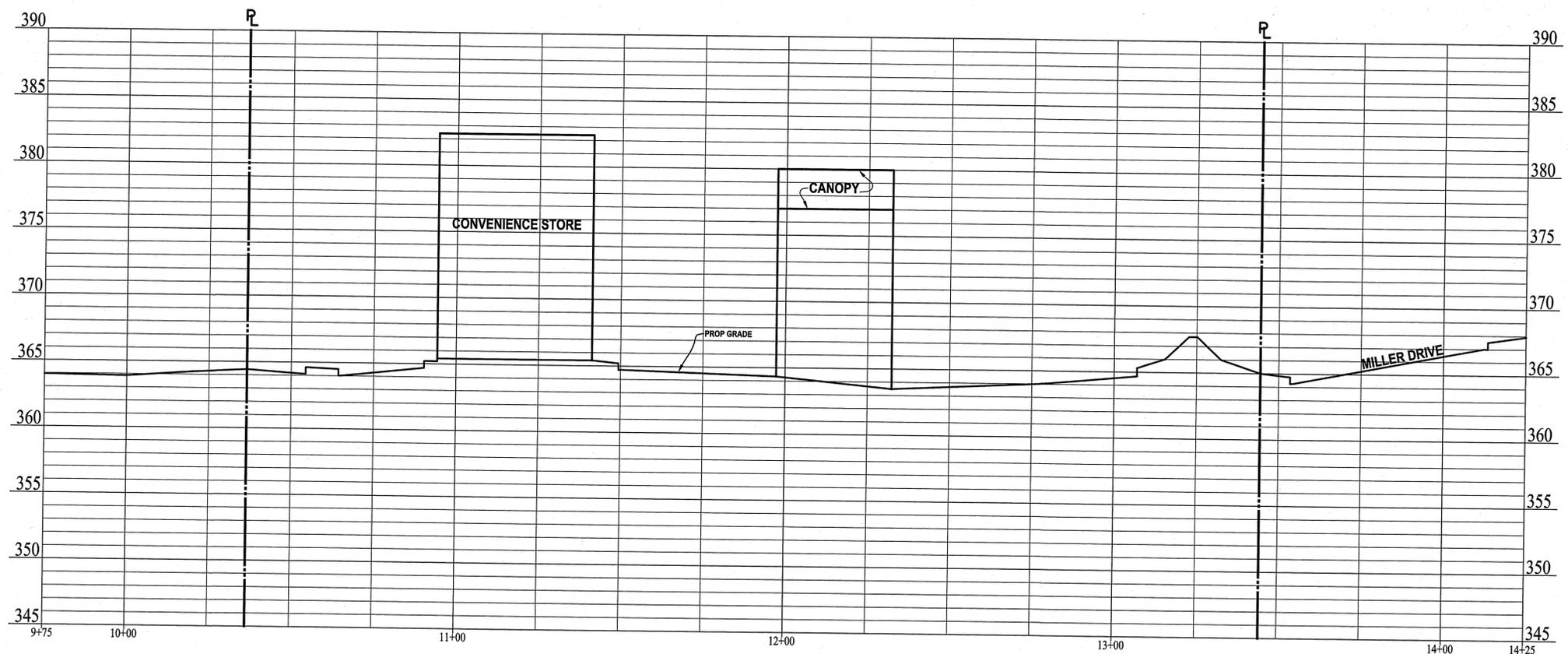


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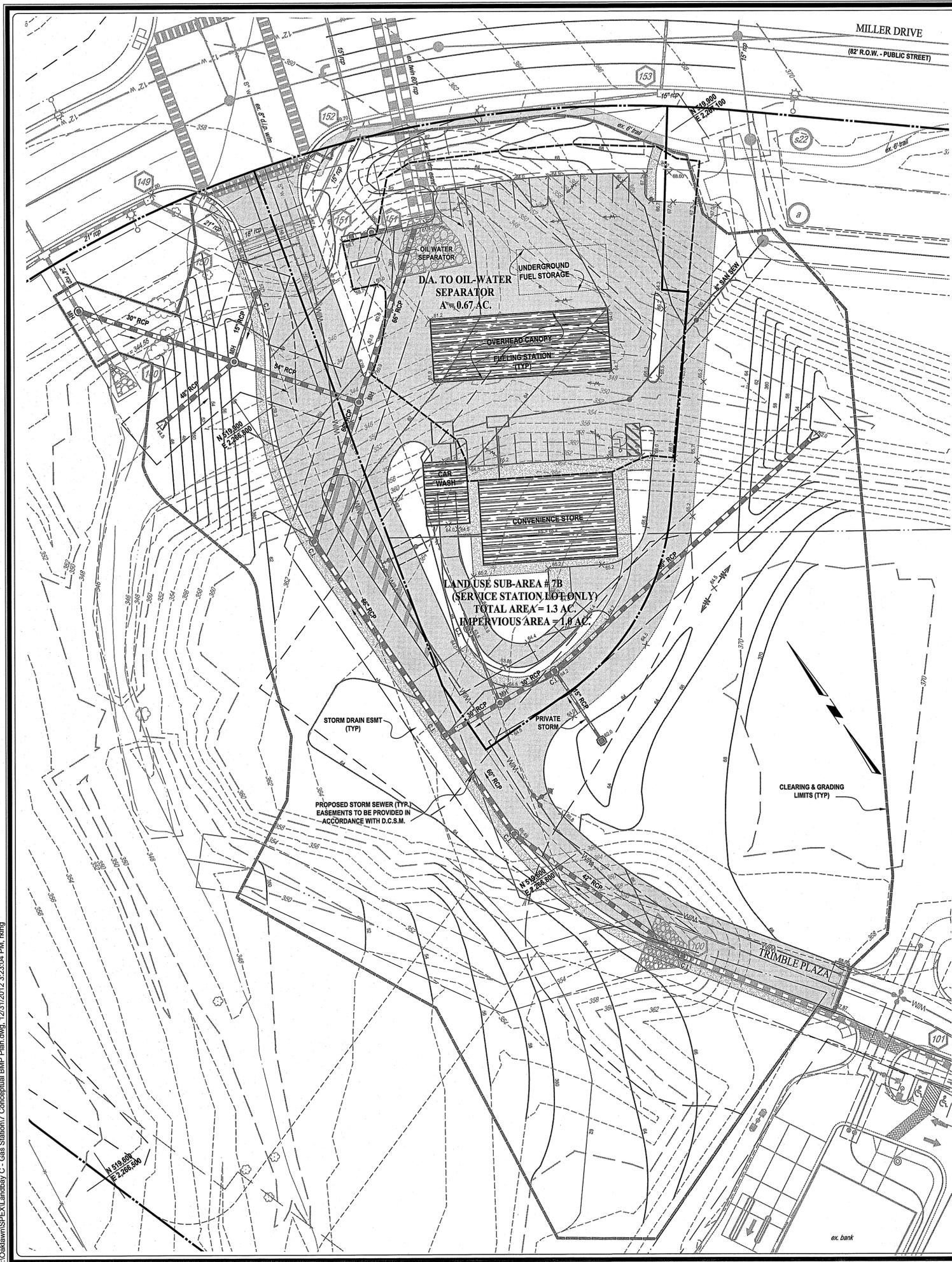
← WEST X-SECTION A EAST →

SEE SHEET 5 FOR X-SECTION LOCATIONS



← SOUTH X-SECTION B NORTH →

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CDS2015-4-C DESIGN NOTES

CDS2015-4-C RATED TREATMENT CAPACITY IS 0.7 CFS (19.8 L/s) OR PER LOCAL REGULATIONS. MAXIMUM HYDRAULIC INTERNAL BYPASS CAPACITY IS 10.0 CFS (283 L/s). IF THE SITE CONDITIONS EXCEED 10.0 (283 L/s) CFS, AN UPSTREAM BYPASS STRUCTURE IS REQUIRED.

THE STANDARD CDS2015-4-C CONFIGURATION IS SHOWN. ALTERNATE CONFIGURATIONS ARE AVAILABLE AND ARE LISTED BELOW. SOME CONFIGURATIONS MAY BE COMBINED TO MEET SITE REQUIREMENTS.

CONFIGURATION DESCRIPTION

GRADED INLET ONLY (NO INLET PIPE)
GRADED INLET WITH INLET PIPE OR PIPES
CURB INLET ONLY (NO INLET PIPE)
CURB INLET WITH INLET PIPE OR PIPES
REINFORCED CONCRETE (R.C.) INLET PIPE REQUIRED (SEE CONFIGURATION)
SEDIMENT WEIR FOR INLET/INLET CONFORMING UNITS

SITE SPECIFIC DATA REQUIREMENTS

STRUCTURE ID	WATER QUALITY FLOW RATE (CFS OR L/s)	PEAK FLOW RATE (CFS OR L/s)	RETURN PERIOD OF PEAK FLOW (YRS)	SCREEN APERTURE (4000 OR 4700)
INLET PIPE 1				
INLET PIPE 2				
OUTLET PIPE				
RIM ELEVATION				
ANTI-FLOTTATION BALLAST	WIDTH	HEIGHT		

NOTES/SPECIAL REQUIREMENTS:

* PER ENGINEER OF RECORD

GENERAL NOTES

- CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
- DIMENSIONS SHOWN WITH (1) ARE REFERENCE DIMENSIONS. ACTUAL DIMENSIONS MAY VARY.
- FOR FABRICATION DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHTS, PLEASE CONTACT YOUR CONTECH ENGINEER.
- CDS WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
- STRUCTURE SHALL MEET AASHTO H-20 AND AASHTO HS-20 (ASHTO M-20) LOAD RATING, ASSUMING GROUNDWATER ELEVATION AT, OR BELOW, THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION.
- PVC HYDRAULIC SHEAR PLATE IS PLACED ON SHELF AT BOTTOM OF SCREEN CYLINDER. REMOVE AND REPLACE AS NECESSARY DURING MAINTENANCE CLEANING.

INSTALLATION NOTES

- ANY EXISTING BACKFILL DEPTH, AND/OR ANTI-FLOTTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE CDS MANHOLE STRUCTURE (LIFTING CLUTCHER PROVIDED).
- CONTRACTOR TO ADD JOINT GROUT BETWEEN ALL STRUCTURE SECTIONS, AND ASSEMBLE STRUCTURE.
- CONTRACTOR TO PROVIDE, INSTALL, AND GROUT PIPES. MATCH PIPE INVERTS WITH ELEVATIONS SHOWN.
- CONTRACTOR TO TAKE APPROPRIATE MEASURES TO ASSURE TIGHT WATER TIGHT, HOLDING WATER TO FLOWLINE INVERT MINIMUM. IT IS RECOMMENDED THAT ALL JOINTS BELOW PIPE INVERTS ARE GROUTED.

CONTECH ENGINEERED SOLUTIONS
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8033 Gates Road, Suite 400, Leesburg, VA 20176
703-777-1232 703-777-7000 703-777-7002

**CDS2015-4-C
INLINE CDS
STANDARD DETAIL**

PLAN VIEW B-B
N.T.S.

ELEVATION A-A
N.T.S.

FRAME AND COVER
(DIAMETER VARIES)
N.T.S.

**STORMWATER RUNOFF QUALITY CONTROL
NARRATIVE**

- General Water Quality requirement compliance per DCSM 5-610 is provided by existing Stratford Lake Facility. (see sheet 8)
- BMP Hot Spot Narrative**
This site will operate as a fuel storage and dispensing station and is considered a Hot Spot per DCSM (5-640.4). A Baysaver or equivalent approved product will be required to contain minor spills on site. The designed system will provide a primary separation manhole, secondary storage manhole (or facility) and Baysaver separator (or approved equivalent). A shutoff valve, easily accessible and signed, is to be installed on the downstream side of Baysaver or equivalent. In the event of spill, the valve is to be closed and the spill contained within the pipes and structures which will be over-sized to hold the contaminated rainwater and material until HAZMAT teams reach the site. Secondary storage and/or storage facility shall be designed to contain a minimum of 2500 gallons.
Notes:
 - BMP Hot Spot structure details to be provided on Final Site Plan.
 - BMP Hot Spot Maintenance Plan to be provided on Final Site Plan.
 - BMP Hot Spot site specific Narrative to be provided on Final Site Plan.
 - BMP Hot Spot structure sizing computations to be provided on Final Site Plan.
 - BMP Hot Spot structure capacities to be provided on Final Site Plan.
 - The Applicant shall provide evidence that an approved Emergency Response Plan has been filed with and approved by the Town as well as the Loudoun County Department of Fire and Rescue Services.

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**OAKLAWN - COMMERCIAL
LANDBAY C**

TOWN OF LEESBURG
LOUDOUN COUNTY, VIRGINIA

CONCEPTUAL BMP PLAN

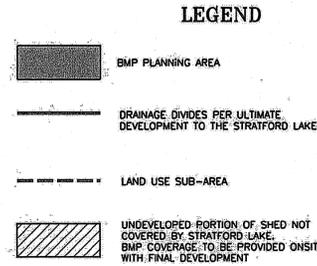
HEALTH OF VIRGINIA
William
SAMUEL JACKSON WILLIAMS, III
Lic. No. 017783
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PROFESSIONAL ENGINEER

DATE: 04-05-2012
FILE NO: L190.012.B
DRN: REKSR
CKD: S.J.W.III

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STRATFORD LAKE FACILITY DRAINAGE SHED AND LAND USE MAP



BEST MANAGEMENT PRACTICES (BMP) NARRATIVE & CALCULATIONS

STRATFORD LAKE ADJOINS THE NORTHERN BOUNDARY OF THE OAKLAWN AT STRATFORD DEVELOPMENT. OAKLAWN HAS ACQUIRED PERMISSION FROM THE OWNERS OF THE STRATFORD LAKE FOR UTILIZATION AS A BMP FACILITY. THE FOLLOWING CALCULATIONS, IN ACCORDANCE WITH THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK SECTION 5-10.2, DEMONSTRATE THAT STRATFORD LAKE MEETS THE REQUIREMENTS FOR A RETENTION BASIN-11 (WITH A TARGET PHOSPHORUS REMOVAL EFFICIENCY OF 50%).

PERFORMANCE-BASED WATER QUALITY CALCULATIONS

STEP 1 - Determine the Applicable Area, (A) and the Post-Developed Impervious Cover, (Ipost).

APPLICABLE AREA, (A):

Total Site Area = 270.1 AC
Planning Area = 263.1 AC
A = 263.1 AC

POST-DEVELOPED IMPERVIOUS COVER, (Ipost):

Tabulation of Impervious Areas	SF or AC	Ipost = Total Impervious Area / A
Buildings	AC	Ipost = 93.8 AC
Pavement	SF or AC	
Driveways	SF or AC	263.1 AC
Sidewalks	SF or AC	35.7%
Total	4,085,928 SF or 93.8 AC	

STEP 2 - Determine the Existing Impervious Cover, (Iexisting) OR Use the Average Land Cover Condition, (Iwatershed)

EXISTING IMPERVIOUS COVER, (Iexisting):

Tabulation of Impervious Areas	SF or AC	Iexisting = Total Impervious Area / A
Buildings	SF or AC	Iexisting = 0.00AC
Pavement	SF or AC	
Driveways	SF or AC	263.1 AC
Sidewalks	SF or AC	0.0%
Total	N/A	

AVERAGE LAND CONDITION, (Iwatershed):

Iwatershed = 16%

Is the Existing Impervious Cover (Iexisting) served by an existing stormwater management BMP that addresses water quality? NO (Assumption is ex. conditions are prior to lake design & construction)

STEP 3 - Determine the Appropriate Development Situation

DEVELOPMENT SITUATION

SITUATION 2

Land development where the Existing Impervious Cover (Iexisting) is less than or equal to the Average Land Cover (Iwatershed) and the proposed improvements will create a total Percent Impervious Cover (Ipost) which is greater than the Average Land Cover Condition (Iwatershed).

REQUIREMENT

The Post-Development Pollutant Discharge (Lpost) shall not exceed the Existing Pollutant Discharge based on the Average Land Cover Condition (Lpre-watershed).

STEP 4 - Determine the Relative Pre-Development Pollutant Load, (Lpre)

Lpre-existing = $[0.05 + (0.009 \times Iexisting)] \times A \times 2.28$
 $[0.05 + (0.009 \times 0.0)] \times 263.1 \times 2.28$
 30.0 LB/YR
 Lpre-watershed = $[0.05 + (0.009 \times Iwatershed)] \times A \times 2.28$
 $[0.05 + (0.009 \times 16.0)] \times 263.1 \times 2.28$
 116.4 LB/YR

STEP 5 - Determine the Relative Post-Development Pollutant Load, (Lpost)

Lpost = $[0.05 + (0.009 \times Ipost)] \times A \times 2.28$
 $[0.05 + (0.009 \times 35.7)] \times 263.1 \times 2.28$
 222.5 LB/YR

STEP 6 - Determine the Relative Pollutant Removal Requirement, (RR)

RRexisting = $Lpost - (0.9 \times Lpre-existing)$
 $222.5 - (0.9 \times 30.0)$
 185.5 LB/YR
 RRwatershed = $Lpost - Lpre-watershed$
 $222.5 - 116.4$
 106.1 LB/YR

RR = $Lpost - Lpre-watershed$
 106.1 LB/YR

STEP 7 - Identify the Best Management Practice (BMP) Options for the Site

REQUIRED REMOVAL EFFICIENCY:

EFF = $(RR / Lpost) \times 100$
 $(106.1 / 222.5) \times 100$
 48%

STRUCTURE	BMP TYPE (SIZE)	Approx. Area (AC)	Impervious Area (AC)	Ipost (%)	Lpost (LB/YR)	EFF (%)	Lremoved (LB/YR)
Lake	Retention Basin	263.1	93.8	36	222.5	50	111.2

Lremoved/total = 111.2 LB/YR
 RR = 106.1 LB/YR

Lremoved/total > RR THEREFORE, BMP REQUIREMENTS MET

IMPERVIOUS AREA TO LAKE TABULATION

#	LAND USE SUB-AREA	TOTAL AREA	AVERAGE (1) IMPERVIOUS AREA	IMPERVIOUS AREA
1	OAKLAWN PARK	4.1 AC.	(2)	0.2 AC.
2	OAKLAWN TOWNHOUSES SECTION 1	3.4 AC.	(2)	1.2 AC.
3	OAKLAWN TOWNHOUSES SECTION 2	1.8 AC.	(2)	1.0 AC.
4	OAKLAWN TOWNHOUSES SECTION 3	3.1 AC.	68% (1)	2.1 AC.
5	PRC MIXED USE (MUC5)	4.2 AC.	(2)	2.2 AC.
6	OAKLAWN (LANDBAY D)	15.3 AC.	(2)	8.6 AC.
6A	HOPE PARKWAY	5.3 AC.	(2)	4.3 AC.
7	OAKLAWN COMMERCIAL (LANDBAY C)	10.9 AC.	80% (1)	8.7 AC.
7A	OAKLAWN COMMERCIAL (LANDBAY C LOT C-3A)	2.4 AC.	(2)	1.4 AC.
7B	OAKLAWN COMMERCIAL (LANDBAY C GAS STATION)	1.3 AC.	(4)	1.0 AC.
8	OAKLAWN COMMERCIAL (LANDBAY A)	14.9 AC.	65% (1)	9.7 AC.
9	OAKLAWN COMMERCIAL (LANDBAY B)	16.9 AC.	65% (1)	11.0 AC.
10	PRC MIXED USE (MUC 2)	11.4 AC.	65% (1)	7.4 AC.
11	OPEN SPACE	23.6 AC.	0% (1)	0 AC.
12	ADDITIONAL ROAD R.O.W.	3.5 AC.	(2)	1.9 AC.
13	DULLES GREENWAY	28.1 AC.	(2)	9.5 AC.
14	FREEDOM PARK	9.8 AC.	(3)	1.8 AC.
15	DETACHED RESIDENTIAL	16.4 AC.	(3)	4.3 AC.
16	SIMPSON MIDDLE SCHOOL	28.0 AC.	(3)	7.0 AC.
17	AIRPORT	23.2 AC.	(3)	0.3 AC.
18	EAST STRATFORD (LANDBAY E)	2.7 AC.	(2)	2.0 AC.
19	EAST STRATFORD (LANDBAY D)	13.5 AC.	(3)	5.0 AC.
20	EAST STRATFORD (PHASE B)	8.6 AC.	(3)	2.8 AC.
21	STRATFORD LAKE	10.7 AC.	(2)	0.4 AC.
TOTAL		263.1 AC.	35.7%	93.8 AC.

- (1) AVERAGE IMPERVIOUS COVER ESTIMATED BASED ON OAKLAWN CONCEPT PLANS.
- (2) IMPERVIOUS AREA SHOWN PER CONSTRUCTION PLANS FOR THE SITE INDICATED IN THE LAND USE COLUMN.
- (3) IMPERVIOUS AREA SHOWN PER GIS DATA AND VISUAL OBSERVATIONS FOR SITE INDICATED IN THE LAND USE COLUMN.
- (4) PER TLSE-2012-0005.

STRATFORD LAKE DESIGN DATA

(PER STRATFORD-RYLAND LAKE FACILITY PLANS PREPARED BY DEWBERRY & DAVIS.)

AT NORMAL POOL:
 ELEVATION = 330.00
 AS-BUILT AREA = 6.07 ACRES (PER DEWBERRY 10-20-03)
 DEPTH = APPROXIMATELY 15% OF LAKE IS 0'-1.5' DEEP. MOST OF LAKE IS 2'-7' DEEP.
 AS-BUILT VOLUME = 31.06 AC.FT. (PER DEWBERRY 10-20-03)

WATER QUALITY VOLUME CALCULATION

(PER SECTIONS 3.06 & 5-6 OF THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK)

WQV = IMP AREA (ft²) * 1/2 in / 12 in/ft
 $= 4,085,928 \text{ ft}^2 * (1/2 \text{ in}) / (12 \text{ in/ft})$
 $= 170,247 \text{ ft}^3$
 $= 3.91 \text{ ac.ft.}$
 $4 * \text{WQV} = 15.63 \text{ ac.ft. (MINIMUM REQUIRED)}$

15.63 ac.ft. < 31.06 ac. ft. (PROVIDED-PER DEWBERRY 10-20-03)

CONCLUSION:

NO MODIFICATIONS TO THE STRATFORD LAKE ARE NECESSARY PER SECTION 3.06 OF THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK. LAKE IS SIZED ADEQUATELY TO ACT AS A RETENTION BASIN-11 WITH A TARGET PHOSPHORUS REMOVAL EFFICIENCY OF 50%.

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**OAKLAWN - COMMERCIAL
 LANDBAY C**

TOWN OF LEESBURG
 LOUDOUN COUNTY, VIRGINIA

**CONCEPTUAL
 STRATFORD LAKE BMP
 COMPUTATIONS**

PROFESSIONAL ENGINEER

12-31-12

SAMUEL JACKSON WILLIAMS, III
 Lic. No. 017783

DATE: 04-05-2012
 FILE NO: L190.012.B
 DRN: REKSR
 CKD: S.J.W.III

II. CURRENT SITE CONDITIONS

The subject property is vacant land located at 501, 524, and 534 Trimble Plaza SE. in the Oaklawn at Stratford Planned Development Community. The proposed lot is 1.3 acres in size located within the part of the site zoned PEC, Planned Employment Center (see figure 2). The property is identified as Loudoun County Parcel Identification Numbers (PINs) 233-20-0550, 233-29-6350, 233-29-9822. The *Town Plan* designates this property as “Regional Office” on the Land Use Policy Map.

Miller Drive provides the public road access to this site. Trimble Plaza is a private travelway that is proposed to connect Miller Drive to Battlefield Parkway through Land Bay C. The southernmost portion of Trimble Plaza (adjacent to Battlefield Parkway) was constructed as part of development of the Northwest Federal Credit Union. The remainder of Land Bay C is vacant although there has been some preliminary grading of the site.

The proposed 1.3 acre site is comprised of portions of three separate parcels in Land Bay C. Should this special exception application be approved, the applicant will submit a revised subdivision plan to record this lot.

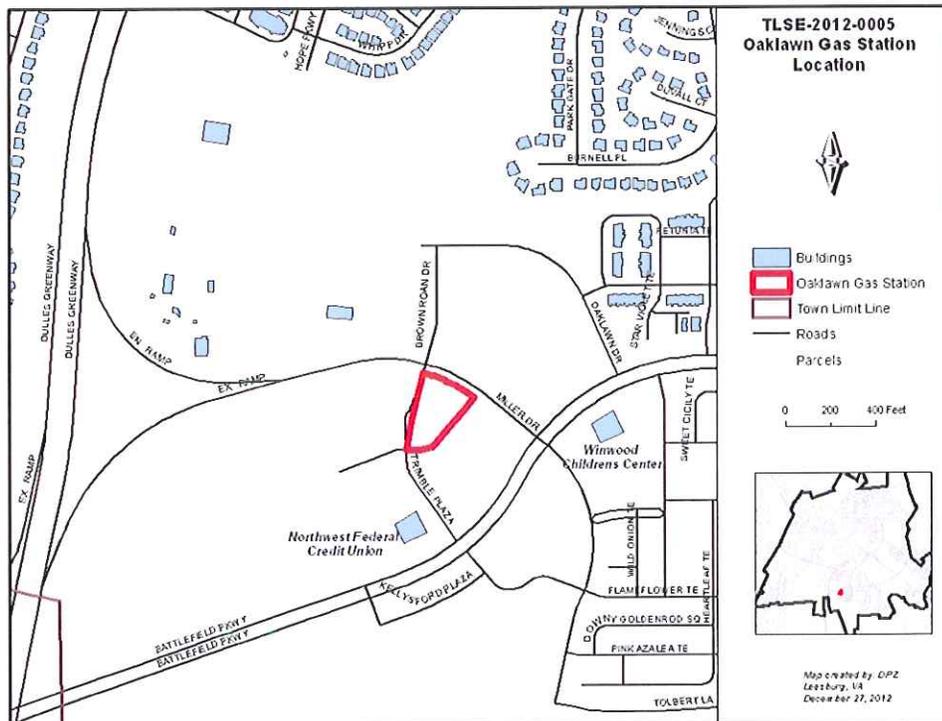


Figure 2. Location

III. ZONING HISTORY

The site is currently zoned PEC (“Planned Employment Center” - see figure 3). In June 2003 as approved in ZM-159 the 448 acre Oaklawn at Stratford planned development was rezoned to PRC (Planned Residential Community) and PEC (Planned Employment Center). In January 2006 a Town Plan Amendment changed the land use designation for Land Bay C and three other land bays from Community Office /Light Industrial to Regional Office. In February of 2007 a Rezoning Concept Plan Amendment and Proffer Amendment (application number TLZM-2005-0002) were approved to permit by special exception an automobile service station with a convenience store and/or car wash as an alternative use in Land Bay C. The amendment only changed the potential location for the service station; the size of the facility was retained as approved in the initial rezoning.

The applicant previously submitted a special exception for a gas station in Land Bay C in 2008 (TLSE-2008-0001). That application was subsequently withdrawn following a Planning Commission public hearing on July 17, 2008. No members of the public spoke at the hearing. The Planning Commission discussed issues related to Regional Office design, the Gateway Corridor designation in the Town Plan and the H-2 guidelines. Concerns were expressed regarding pedestrian access, multiple points of entry to the site, and visibility of the canopy and pumps from Miller Drive. The Commission also questioned the architectural design. The commission then voted to recommend denial of the special exception request on a 4-1-2 vote. Rather than moving forward for Town Council action, the applicant withdrew the special exception request.

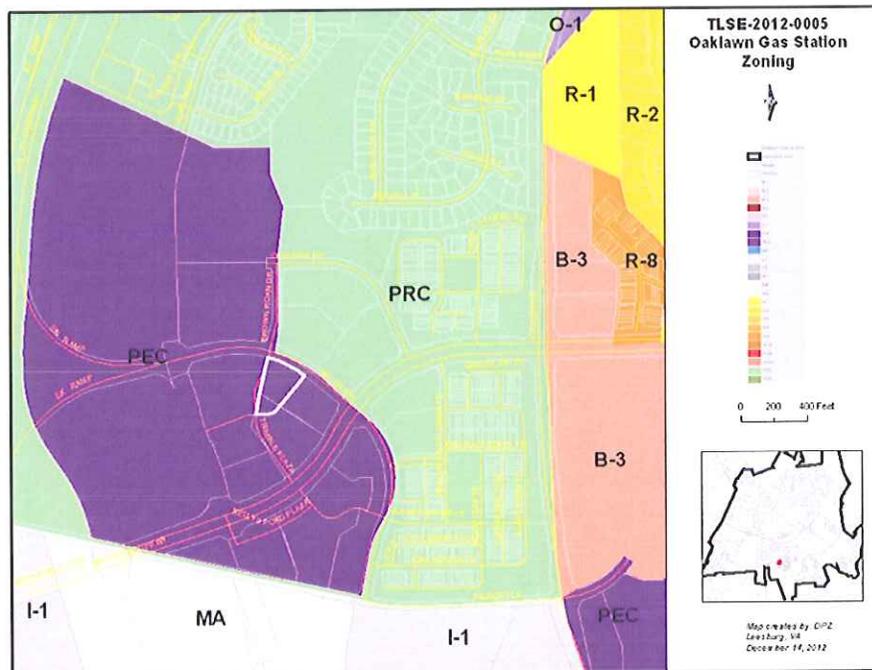


Figure 3. Zoning

IV. STAFF ANALYSIS

A. **Town Plan Compliance:** The Town of Leesburg Zoning Ordinance (TLZO) Section 3.4.12 requires an assessment of how the proposed special exception use will comply with applicable provisions of the *Town Plan*.

1. **Proposed Use:** The property is designated “Regional Office” (see figure 4). The Town Plan describes regional office as designed for a broad spectrum of local and regional employment as well as provision of daily retail and service needs of people associated with the primary office use in a manner that reduces automobile uses (Intent Statements #1 and 2, page 6-24). The “Uses” section for Regional Office does not specifically mention a gas station, convenience store, and car wash as appropriate (Town Plan, p. 6-25). However, in 2007 the Town approved TLZM-2005-0002, a concept plan and proffer amendment allowing the gas station, car wash, and convenience store in Land Bay C as support services to the primary office use. For this reason, staff considers the proposed uses acceptable as related to the Town Plan.

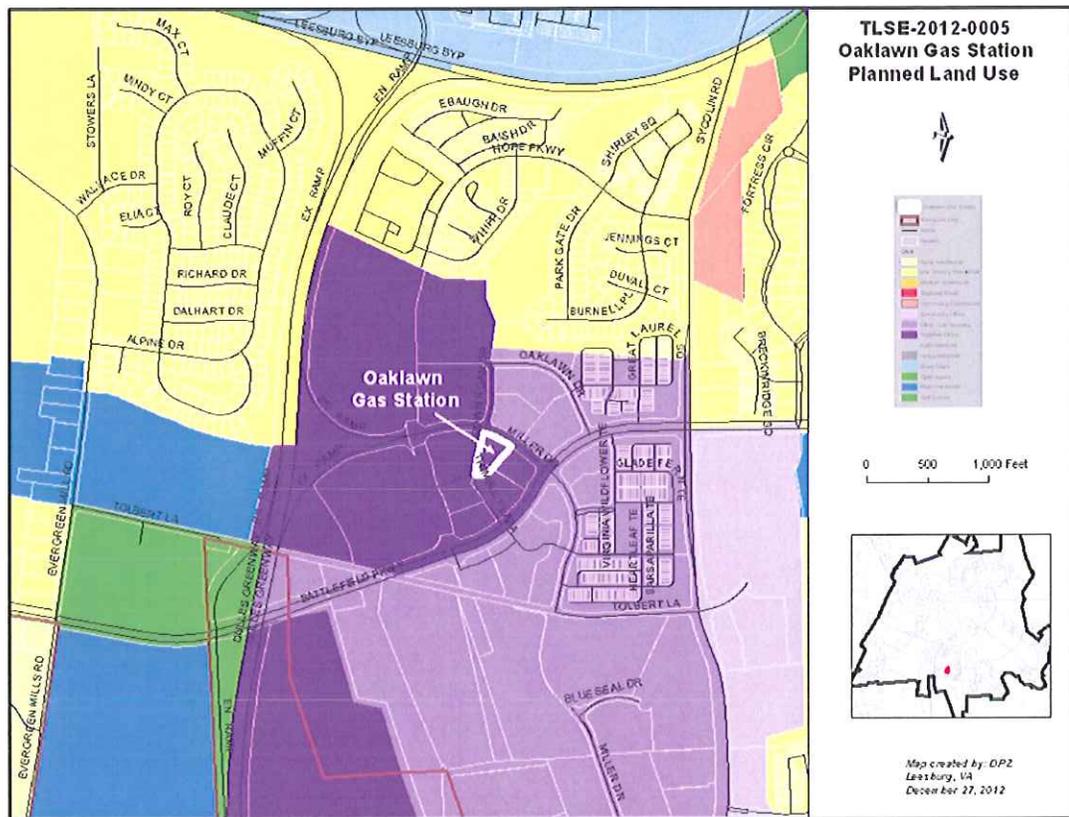


Figure 4. Planned Land Use

- 2. Gateway Corridor:** The Town Plan indicates that Regional Office development should be compatible with Leesburg's character as established by the more traditional urban core (Intent Statement #5, page 6-24). The proposed design has the rear of the convenience store facing Trimble Way with the large overhead canopy being the most visible element from the street. This is a conventional suburban gas station design rather than traditional town design expected for a Gateway Corridor in a development planned as Regional Office. Staff's assessment is that the site layout does not meet the Town Plan Gateway Corridor guidance.

The applicant has responded that the proposed use is auxiliary to the primary regional office park uses and requires special site design considerations. They argue that the service station's layout is dictated by its operational needs.

Staff has provided the applicant with examples of existing development where this type of use is developed in a manner that is more consistent with Gateway Design corridors (see example below in Figure 5). However, the applicant has stated they are unwilling to alter the site layout. They have indicated that their experience with service stations shows that for operational ease the car wash and pumps must both be easily accessible to employees from the convenience store in the manner proposed by their design. Staff notes there are numerous examples locally and nationally of alternative designs including ones where the car wash is not adjacent to the convenience store (see Figure 6 as an example).

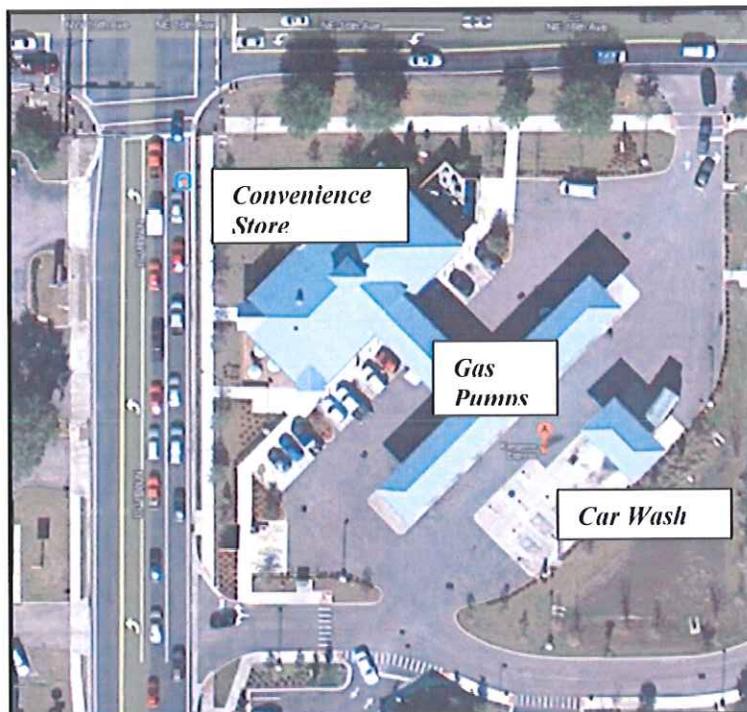


Figure 5. Alternative Site Layout Example



Figure 6. Gas Station in Leesburg showing separate car wash

3. **Site Design:** The Town Plan states “site design should conform to H-2 district design guidelines” (Site Design and Location Criteria #6, p. 6-25). Staff’s assessment of the site design indicates that this does not meet H-2 design guidelines for building size, scale, and orientation nor for building appearance. While building appearance is addressed at the time of site plan, the size, scale, and orientation of the building must be addressed now during the special exception process (otherwise, a site plan would not be in “substantial conformance” with the approved special exception plat). A more thorough discussion of the H-2 District Design issues is provided under item “B” of this Staff Analysis section of this report. An example of an alternative site layout that meets H-2 Guidelines is shown in figure 5.
4. **Staff Findings Related to the Town Plan:** Both in the review of the similar 2008 application and for this special exception, staff has consistently identified the siting and design issues as not conforming to the Town Plan guidance. Staff requested that the applicant reconfigure the site layout so that the convenience store engaged one of the adjacent roads (Trimble Way or Miller Drive) and relocate the car wash to a less visible area of the site. Although the applicant has never provided proof of consideration of alternative designs, they maintain that their operational needs prevent them from meeting the design policies. Staff does not believe operational objectives and site design guidelines are mutually exclusive; it is possible to achieve both. No evidence has been provided to show operations can not work in harmony with design guidelines. In summary, the proposal is not in conformance with the Town Plan design guidelines and staff has seen nothing

that suggests that site design as prescribed by the Town Plan and H-2 Design guidelines cannot be achieved while also meeting operational needs.

- B. H-2 District Design Guidelines:** The Town Plan guidance is not the only directive citing H-2 guidelines. The applicant's proffers also commit this site to review for conformance with the H-2, Historic Corridor Architectural Control Overlay District Design Guidelines (TLZM-2005-0002, proffer #V.20).

Staff has identified several areas in conflict with the proffered H-2 Corridor Design Guidelines including, but not limited to, the siting of the buildings, the massing and height, pedestrian access and screening (see attachment 4) further described below.

1. ***Building Orientation:*** The "siting/relationship to roadway" guidelines for H-2 indicate that the building should be oriented toward and have direct pedestrian access to the roadway from which it is accessed (in this case, Trimble Way). Primary buildings should not be placed behind parking facing the entry. With the current site plan, the car wash could be relocated toward the landscape buffered part of the site adjacent to Miller Drive with appropriate architectural treatments. As well, parking areas should not be located exclusively at the front of building sites.

The Applicant has responded that the site layout is a customary configuration for the proposed uses of convenience store, gas pumps and car wash. They indicate that the operational demands require employees based in the convenience store to be able to see the gas pumps as well as being proximate to the car wash building. Staff acknowledges that the H-2 guidelines do not fit the particular operational desires of the applicant, but the applicant has provided no compelling reason for not conforming to the guidelines.

2. ***Site Access:*** (H-2 Corridor Design Guidelines, pp. 32-33): To minimize traffic conflicts, vehicular entrances/exits to a site should be consolidated to a single location. Further, "entrances and driveways should permit safe and convenient pedestrian crossing where they intersect sidewalks and other pedestrian circulation systems." In this case, the proposal has three two-way access points on a small lot creating conflicts with pedestrians who will be walking to this site and to other future uses in this Land Bay. Fewer entrances and more uninterrupted sidewalks would better achieve the H-2 design guidelines or the design goals of Regional Office.
3. ***Pedestrian Safety:*** H-2 guidelines call for safe and convenient pedestrian access. The pedestrian access as proposed from across Trimble Plaza is awkward, long and wraps around the rear of the building requiring pedestrians to cross the 35-foot wide car wash entrance aisle. Staff has recommended an alternate site layout with the convenience store facing Trimble Way. That alternative site layout would facilitate direct, safe, and convenient pedestrian access.

4. **Parking:** Parking for this site is exclusively at the front of the site inconsistent with the parking lot guidance for the H-2 District. (H-2 Corridor Design Guidelines, pp. 34). Again, an alternative site layout could address this issue but the applicant has expressed an unwillingness to make the change due to operational desires.
5. **Building Appearance:** Because they face private internal travelways that function as roads, the side and rear facades of the convenience store should be designed with as much attention as the primary façade. (*Note: This can be addressed during review of the actual Certificate of Appropriateness and is not required at this time.*)

Board of Architectural Comments

The Board of Architectural Review (BAR) reviewed this project for consistency with the H-2 guidelines on November 19, 2012. Some members of the BAR expressed concern with the site layout as proposed. However, the BAR stopped short of providing any specific site layout changes. Other comments were made regarding the architecture, but architectural comments will be dealt with later as part of the actual Certificate of Appropriateness review.

In summary, given the proposed site layout's nonconformance with the design guidelines described above, staff finds the proposal does not meet the H-2 standards as of TLZO 7.6.7 nor the H-2 Design Guidelines.

- C. **Use/Compatibility with Adjacent Properties:** The site is located internal to a largely vacant area of a planned development community as such there are no incompatibility issues (see table below).

Direction	Existing Zoning	Current Use
North	PEC/PRC	Vacant
South	PEC	Vacant
East	PEC	Vacant
West	PEC	Vacant

- D. **Transportation:** A conditional waiver of the traffic study requirement was granted due to the fact that there is an adequate road network to handle the vehicle trips generated solely by this use. Site ingress and egress will occur from private travelways within Landbay C; no direct access will occur from the site onto Miller Drive or Battlefield Parkway. As such, the existing public roads are adequate to handle traffic generated by the proposed gas station, convenience store, and car wash.

However, staff notes that there are similarities between the proposed major access point for this gas station site (proposed adjacent to Miller Drive) and the existing Sheetz gas station located adjacent to Edwards Ferry Road. In each of these examples, a two-way private travelway with a single lane in each direction and a minimal left turn lane into the site serves a gas station. There have been repeated

stacking and access issues for vehicles at the Sheetz station. While there are sufficient road improvements now for the proposed gas station facility here at Oaklawn, staff is concerned that another "Sheets" situation (i.e. excessive stacking in the public right of way) could occur in the future as this site is fully developed.

One way to avoid this possibility is for the land bay to be served by two lanes into the site from Miller Drive (onto Trimble Plaza) with the innermost lane dropping off as a left turn only lane into the proposed gas station. The outer lane would then enhance internal vehicular circulation by serving as a bypass lane for access to the rest of the landbay which would minimize the turning movement conflicts that potentially could cause excessive stacking in the Miller Drive public right of way. This need for this type of improvement will be further analyzed in a future traffic study to be provided as the rest of the land bay develops.

On the positive side, this site (unlike Sheetz) has more than one point of access to the landbay. In the future, there will be three points of ingress/egress to the landbay (Trimble Plaza connects to Battlefield Parkway and a future extension of Hope Boulevard will also lead to Landbay C).

The Traffic Study Waiver issued on June 14, 2012 (see Attachment 5) finds that the subject intersection will meet acceptable levels of service for the proposed service station/car wash/convenience store uses. To provide an opportunity to address this situation in the future should it arise, the traffic study waiver granted for this case contained a condition that states "Future land development within Landbay C will impact this intersection [Miller Drive/Trimble Plaza]" and that as a result the Town will require a traffic study for future development proposals beyond the subject service station/car wash/convenience store within Landbay C.

E. Utilities: The Department of Utilities has no principal objections to the applications at this time but noted that there are comments that must be addressed at the time of site plan review. (Attachment 8 from Deputy Director of Utilities).

V. Specific Use Standards per Zoning Ordinance: The Zoning Ordinance (TLZO) provides specific use standards for convenience stores, service stations and car wash uses under Article 9 below.

Section 9.3.3 Car Washes

A. *"Car washes shall be located and designed so that vehicular circulation shall not conflict with traffic movements in adjacent streets, service drives, driveways and/or parking areas."* - Staff believes the present design with a separate entrance from off-site from the convenience store and gas pumps to the car wash and three two-way entrances and exits elsewhere on site is not optimal and could be redesigned to reduce potential conflicts. To enter the car wash a vehicle must drive off-site into an adjacent travelway and then reenter the site at a fourth entrance, this one exclusive for the car wash. Future development of the remainder of the Land Bay will only

increase the potential for conflicts between vehicles moving around the site.

- B. *Car washes (as an accessory use) shall have no separate and exclusive curb cut access to the abutting roadway.* – The Applicant’s proposal technically complies with this standard but there is an exclusive entrance into the site for the car wash from an off-site travelway.

Section 9.3.8 Convenience Food Stores

- A. *No convenience food store shall be located on a lot adjacent to residential dwelling units.* - Applicant’s proposal complies with this standard.
- B. *All business services, storage and display of goods shall be conducted within a completely enclosed building.* - Applicant’s proposal complies with this standard.

Section 9.3.24 Service Stations

- A. *Service stations shall not include ancillary uses such as vehicular or tool rental (including moving vans) and shall be limited to the servicing of vehicles and non-automotive accessory retail sales of snacks, convenience foods, and similar products.* - Applicant’s proposal complies with this standard. Applicant does not propose any service bays.
- B. *Service stations shall not include an outdoor storage area for more than three (3) abandoned, wrecked or inoperable vehicles on the site for more than one week, subject to the limitation that there shall be no dismantling, wrecking, or sale of said vehicles or part(s) thereof.* - Applicant’s proposal complies with this standard.
- C. *Outside sales and display areas shall be shown on the plan to be reviewed by the Planning Commission and Town Council.* - Applicant proposes no outside sales or display areas.
- D. *The parking of commercial vehicles shall be prohibited on the site of a service station. This provision shall not be interpreted as prohibiting the parking of vehicles actively engaged in delivering fuel or other supplies to the service station.* - Applicant shall comply with this standard.
- E. *No permit shall be issued for a service station if the entrance or exit for vehicles is:*
 - 1. *In the same block front within 200 feet of any school, public playground, hospital, church, or public library;* - Applicant’s proposal complies with this standard.
 - 2. *If such entrance or exit is located within 20 feet of an “R” District within the same block front;* - Applicant’s proposal complies with this standard.

3. *No permit shall be issued for a service station if any part of any structure, including underground gasoline tanks or service aisles, is located within 100 feet of any building or grounds of a school, public playground, hospital, church, or public library or any "R" District.* - Applicant's proposal complies with this standard.

F. *Canopy height measured from the finished grade to the lowest point on the canopy fascia shall not exceed 14'0". The overall height of canopies shall not exceed 17'3".* – The Applicant has submitted a modification request for this standard (see discussion under "Modifications.")

G. *Canopy lighting shall be fully recessed in the ceiling of the canopy.* - Applicant's proposal complies with this standard.

H. *All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, coloring and detailing.* - Final design of the buildings must be approved by the Board of Architectural Review under the H-2 Corridor Design Guidelines.

VI. Site Design:

Parking. Convenience Store: 6 per 1,000 square feet gross floor area (21 total)
Car Wash: 1.0 per employee, plus required stacking spaces(0)
Fuel Islands (Service Station): 1 per employee(0)

21 parking spaces are required plus 7 car wash stacking spaces.
23 parking spaces are provided per the special exception plat.

Stacking Spaces. TLZO Section 11.8.2 requires a minimum of seven (7) stacking spaces per car wash bay. This proposal meets this standard.

Architectural Elevations. The applicant will need to obtain Board of Architectural Review (BAR) approval pursuant to proffer #V.20 of #ZM-159.

Landscaping/Buffering. A 12.5-foot wide buffer with an S-2 Screen is provided along the frontage of Battlefield Parkway in accordance with the with the rezoning concept plan requirements of #ZM-159. Submission and review of the final site plan application will finalize arrangement and selection of plant materials. The applicant shall provide interior and exterior parking lot landscaping as required by ordinance.

Lighting. Site and Canopy lighting is subject to review and conformance with the DCSM. Foot candles at the property shall not exceed .5 and the source of illumination shall not be visible. Canopy lights are recessed per TLZO 9.3.21.G

to prevent them from being visible offsite. The B.A.R. can also address this issue as part of its review of this proposal.

Storm Water Management. Storm Water Management and Best Management Practices for storm water quality must be in accordance with Loudoun County and Town of Leesburg criteria in force at the time of site plan approval. DPR staff have indicated that due to the proposed gas station use the site is a “BMP Hotspot.” Stormwater management issues will be addressed at the time of site plan.

Hours of Operation. The hours of operation for this facility are intended to be 24 hours a day, seven (7) days a week.

Signs. Applicant will provide signage in accordance with the requirements of TLZO Article 15 Signs.

Pedestrian/Vehicular Accessibility. Applicant provides pedestrian walkways along three sides of the site with the western side served by a sidewalk across the entrance drive on an adjacent parcel. Pedestrian access to the convenience store from Trimble Way is awkward as proposed due to the location of the car wash. Staff requested an alternative design to address this issue but the applicant is unwilling to alter the site layout at this time.

VII. Modifications

Request: Pursuant to TLZO 3.4.13, the applicant requests a modification by the Town Council of TLZO 9.3.24.F a use standard for the canopy height at service stations. This use standard states that “*Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".* The applicant is requesting canopy height as measured from the average finished grade to the lowest point on the canopy fascia not exceed 17'0" while the overall height of the canopy not exceed 20'6".

The applicant’s justification states that the increase is necessary as a safety factor so that semi-tractor trailers may pass beneath the canopy. Conceptual grading plans have the site sloping downward to the east with a difference of up to 3' 6". Because the Zoning Ordinance states that height is measured from the average (emphasis added) grade around the entire structure, the canopy will need a greater height at the lower end in order to remain level. Having an appropriate canopy height with adequate clearances is necessary as a safety factor in the operation of the service station.

Modification Standards: TLZO Section 3.4.13 states that the criteria for evaluation of a use standard modification request is whether Town Council determines that the modification is “necessary to accommodate reasonable use of the site.”

Staff Analysis and Recommendation: The maximum legal truck height is 13'6". The use standard that a canopy fascia be at least 14'0" in height is in place to keep the fascia relatively low while allowing trucks to pass safely underneath. Staff agrees that an increase in the minimum canopy fascia height is needed onsite based on the proposed grades in order to provide a safety factor for trucks. Staff also agrees that some modification of overall height is warranted to accommodate reasonable use of the site given the proposed grades (although staff would encourage the applicant to have a less severe grade under the canopy).

Staff recommends the following modification to Zoning Ordinance Use Standard TLZO 9.3.24F for the purpose of safety:

"Canopy height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0" except as necessary to allow the canopy to remain level where the finished grade of the land slopes beneath the canopy. The final grade on the land beneath the canopy shall not slope more than a total of three and one-half (3.5) vertical feet. The height of the canopy shall not exceed 3'3" above the lower edge of the canopy fascia at any one point and the maximum height of the canopy at any one point shall not exceed 20'9"."

VIII. Proffers:

The site is subject to the applicable proffers of TLZM-2005-0002 Oaklawn (and amendment to the original #ZM-159 Oaklawn case). The Applicant is seeking special exception approval. As a result, the proffers can not be amended through this process. However, conditions to mitigate adverse impacts of the proposed use may be imposed by Council in accordance with TLZO Sec. 3.4.14.

IX. Analysis and Recommendation:

Staff has provided examples of alternative site layouts that could meet both the design objectives and operational needs. The applicant has not demonstrated that they have considered alternative site designs. Nor have they shown how the design guidelines prohibit successful operation of a service station, convenience store, and car wash. For these reasons, staff recommends **denial** of Special Exception TLSE-2012-0005 because the application does not comply with the following:

- Town Plan Regional Office criteria specifying that site design should comply with H-2 District Design Guidelines.
- Regional Office Gateway Corridor design criteria in the Town Plan.

Should the Planning Commission find it appropriate to recommend approval of this application, staff suggests the following minimum conditions:

1. Substantial Conformance. This special exception shall be in substantial conformance with the special exception plat entitled "Oaklawn-Commercial Land Bay C", Sheets 1-8, revised through 12-31-2012 and prepared by Paciulli Simmons & Associates.
2. No Waivers Expressed or Implied. With the exception of the modification listed below, approval of this special exception does not express or imply any waiver or modification of the requirements set forth in the Subdivision and Land Development Regulations, the Zoning Ordinance, or the Design and Construction Standards Manual, except as may have been approved as part of this application. Final plats, site plans, and construction drawings are subject to applicable town regulations.
3. Modification Granted. Modification to TLZO 9.3.24F (Canopy Height) is hereby granted as follows:

"Canopy height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0" except as necessary to allow the canopy to remain level where the finished grade of the land slopes beneath the canopy. The final grade on the land beneath the canopy shall not slope more than a total of three and one-half (3.5) vertical feet. The height of the canopy shall not exceed 3'3" above the lower edge of the canopy fascia at any one point and the maximum height of the canopy at any one point shall not exceed 20'9."

4. Mechanical Equipment. All mechanical equipment, including roof-top equipment, shall be screened from public view by appropriate architectural elements.
5. Building Elevations. The structures shall be by the Board of Architectural Review (BAR) in accordance with Proffer #V.20 of #ZM-159.
6. Building Facades. All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, coloring and detailing.
7. Traffic Signage The applicant shall install a "Do Not Block Intersection" sign for northbound traffic on Trimble Way prior to the main entrance to the gas station.

X. Sample Planning Commission Draft Motions

Denial

Special Exception Use

I move that Special Exception Use Application TLSE-2012-0005, Oaklawn Gas Station, be forwarded to the Town Council with a recommendation of denial on the basis that the Approval Criteria of Zoning Ordinance Section 3.4.12 have not been satisfied for the following:

- Town Plan Regional Office criteria specifying that site design should comply with H-2 District Design Guidelines.
- Regional Office Gateway Corridor design criteria in the Town Plan.

Approval

Special Exception Use

I move that Special Exception Use Application TLSE-2012-0005, Oaklawn Gas Station, be forwarded to the Town Council with a recommendation of approval subject to the conditions of approval contained in the Staff Report dated January 17, 2013 on the basis that the Approval Criteria of Zoning Ordinance Section 3.4.12 have been satisfied and that the proposal would serve the public necessity, convenience, general welfare and good zoning practice; and further recommend approval of the following modifications:

- A modification of TLZO 9.3.24F (Canopy Height) to allow the maximum height of the canopy at any one point to not exceed 20'9."

Approval, with revised conditions

Special Exception Use

I move that Special Exception Use Application TLSE-2012-0005, Oaklawn Gas Station, be forwarded to the Town Council with a recommendation of approval subject to the conditions of approval contained in the Staff Report dated January 17, 2013, as amended by the Planning Commission on January 17, 2013, on the basis that the Approval Criteria of Zoning Ordinance Section 3.4.12 have been satisfied and that the proposal would serve the public necessity, convenience, general welfare and good zoning practice; and further recommend approval of the following modifications:

- A modification of TLZO 9.3.24F (Canopy Height) to allow the maximum height of the canopy at any one point to not exceed 20'9."

Attachments

1. Page 1 Special Exception Plat
2. Written Statement of Justification
3. Modification Request
4. Preservation Planner/BAR memo dated November 21, 2012
5. Conditional Traffic Study Waiver dated June 14, 2012
6. Department of Plan Review memo dated November 20, 2012
7. DPZ Zoning referral memo dated November 19, 2012
8. Department of Public Works memo dated November 19, 2012
9. Utilities memo dated November 1, 2012

Oaklawn, Landbay C
Special Exception Application for an
Automobile Service Station, Convenience Store and Car Wash
Written Statement
April 3, 2012
Revised October 12, 2012

I. Introduction

Oaklawn Development Partners, LLC is filing a special exception application for an automobile service station, convenience store and car wash on 1.15 acre tract of land within Land Bay C of the Oaklawn Planned Employment Community (PEC) development. The 1.15 acres currently is part of 3 separate parcels that will be consolidated into one parcel subsequent to the approval of the proposed special exception application. The Oaklawn PEC district is subject to the proffers for ZM #159 and TLZM 2005-0002. The Oaklawn PEC has been planned for two service stations within the community; however, the proffers for TLZM 2005-0002 stipulated that a special exception would be required if one the of the service stations were to locate in Land Bay C.

II. Type of Operation

A service station is proposed with eight fueling stations containing 16 gas pumps. Along with the service station will be a 3,525 square foot convenience food store and an 825 square foot car wash.

III. Hours of Operation

This facility will operate 24 hours a day, seven days a week.

IV. Traffic Impacts

The traffic studies submitted for ZM #159 and TLZM 2005-0002 analyzed the proposed special exception use. The most recent traffic study analyzed a gas station in Land Bay C as one of the two gas stations within the Oaklawn development. Oaklawn has proffered and phased road improvements based on this study. The impacts of the proposed special exception uses were accounted for in these prior studies and in the proffered and phased road improvements.

V. Impacts on adjacent uses and measures proposed to mitigate such impacts.

The governing Oaklawn PEC zoning with concept plan and proffers created a comprehensively planned community that accounted for a variety of uses needed to create a thriving employment center. Two gas stations always have been contemplated within Oaklawn. In Land Bay C, the proposed special exception site is surrounded by future uses that could include office, light industrial, data center, free-standing restaurant, fast food with drive through and bank with drive-through. One of the Mixed-Use Center land bays is located across Miller Drive from the proposed special exception uses, which is

planned for similar uses proposed in this application, as well as office and residential uses. Oaklawn has been planned with a unified landscaping plan along the public streets, and the special exception uses will comply with the unified landscaping plan along Miller Drive. The uses internal to Land Bay C are intended to be integrated with one another using a coordinated private driveway network.

VI. Compliance with Section 3.4.12 Approval Criteria.

A. The proposed use will not adversely affect the use of neighboring properties.

An amendment to TLZM 2005-0002 in 2007 relocated the 2nd gas station from MUC 2 to Land Bay C for the purpose of moving it further from the residential units in Oaklawn. This proposed Special Exception fulfills the commitment of that proffer amendment. As for adjacent uses, the gas use is fully compatible with the other planned uses such as office, retail, bank, restaurant, pharmacy and personal service.

B. The use shall comply with applicable zoning district regulations and applicable provisions of the adopted *Town Plan*, including but not limited to the Plan's Land Use Compatibility policies.

Land Bay C within Oaklawn is designated for regional office use on the Planned Land Use Map of the Town Plan. Oaklawn is a planned employment community for which the concept plan accommodated locations for the primary uses, as well as the supporting uses. The concept plan for Land Bay C, in particular, designated restaurant, bank and service station uses along with the office uses as part of this regional office community. The regional office designation contemplates the need for retail and service uses for the daily needs of the workers, customers and businesses in the development. This special exception use proposes to provide such uses as contemplated in the concept plan for Oaklawn. Development within Oaklawn is subject to the H-2 Corridor Design Guidelines to ensure architectural compatibility among the various uses within Oaklawn.

C. The location, size and height of buildings, structures, walls and fences, and the nature and extent of screening, buffering and landscaping shall be such that the use will not hinder or discourage the appropriate development and use of adjacent or nearby land and/or buildings.

The proposed uses have been planned as part of the other Land Bay C uses, which include an office building, restaurant and pharmacy uses surrounding the proposed uses. The internal private street network accommodates these uses, and none of the uses interfere with each other's operations. The proposed buildings will be one-story, and the service station canopy will comply with the Zoning Ordinance maximum height requirement. Oaklawn has a unified landscape plan bordering all of the public streets within Oaklawn. The site containing the proposed uses

fronts on Miller Drive, and the Miller Drive landscaping will be provided in accordance with the required buffer and the Oaklawn landscape plan. A six-foot wide trail also will be constructed within the setback along Miller Drive as part of the Oaklawn trail system.

D. The proposed use will be such that pedestrian and vehicular traffic generated will not be hazardous or conflict with the existing and anticipated traffic in the neighborhood and on the streets serving the site.

Both the vehicular and pedestrian access is part of the networks planned for the Oaklawn planned development. The proposed special exception uses have been contemplated as part of the Oaklawn planned development from its inception; therefore, the traffic generated by these uses was anticipated in the initial planning for Oaklawn.

VII. Compliance with Section 9.3, Use Standards

A. Section 9.3.3 Car Washes

A. Car washes shall be located and designed so that vehicular circulation shall not conflict with traffic movements in adjacent streets, service drives, driveways and or parking areas.

The car wash entrance is accessed from an internal driveway that serves this use. The vehicles exiting the car wash will use the site entrance that accesses the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Car washes (as an accessory use) shall have no separate and exclusive curb cut access to the abutting roadway.

The proposed accessory car wash use has an access point from a driveway and vehicles exiting the car wash will use the site's entrance onto the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Section 9.3.6 Convenience Food Store

A. No convenience food store shall be located on a lot adjacent to residential dwelling units.

There are no dwelling units located on the lots adjacent to the proposed special site.

C. All business services, storage and display of goods shall be conducted within a completely enclosed building.

The applicant shall adhere to this requirement once the convenience food store is operating.

D. Section 9.3.21 Service Stations

- A.** *Service stations shall not include ancillary uses such as vehicular or tool rental (including moving vans) and shall be limited to the servicing of vehicles and nonautomotive accessory retail sales of snacks, convenience foods, and similar products.*

Vehicle servicing is not proposed with this application nor is vehicle rentals. Retail sales will occur as part of the convenience food store use.

- B.** *Service stations shall not include an outdoor storage area for more than three (3) abandoned, wrecked or inoperable vehicles on the site for more than one week, subject to the limitation that there shall be no dismantling, wrecking, or sale of said vehicles or part(s) thereof.*

There will be no inoperable vehicles, since servicing of vehicles is not proposed with this service station.

- C.** *Outside sales and display areas shall be shown on the plan to be reviewed by the Planning Commission and Town Council.*

Outside sales and display are not proposed with this use.

- D.** *The parking of commercial vehicles shall be prohibited on the site of a service station. This provision shall not be interpreted as prohibiting the parking of vehicles actively engaged in delivering fuel or other supplies to the service station.*

The applicant will comply with this requirement.

- E.** *No permit shall be issued for a service station if the entrance or exit for vehicles is*

1. *In the same block front within 200 feet of any school, public playground, hospital, church, or public library; or*

None of these uses are included in Oaklawn.

2. *If such entrance or exit is be located within 20 feet of an "R" District within the same block front.*

The entrance for the service station is internal to Land Bay C, which does not contain any R District zoning.

3. *No permit shall be issued for a service station if any part of any structure, including underground gasoline tanks or service aisles, is located within 100*

feet of any building or grounds of a school, public playground, hospital, church, or public library or any "R" District.

None of these uses exist within Oaklawn.

- F.** *Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".*

A modification request pursuant to Section 3.4.3 of the Zoning Ordinance has been submitted to modify the canopy height fascia not to exceed 17'0" and the overall height not to exceed 20'6".

- G.** *Canopy lighting shall be fully recessed in the ceiling of the canopy.*

The proposed canopy lighting will comply with this requirement.

- H.** *All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, color and detailing.*

Oaklawn is subject to the H-2 Corridor Design Guidelines and process. Those guidelines and process can ensure that this provision will be adhered to by the proposed uses.

**TLSE 2012-0005
Oaklawn Commercial Landbay C
Automobile Service Station with Car Wash and Convenience Retail**

ZONING ORDINANCE MODIFICATION REQUEST

**October 12, 2012
Revised January 2, 2013**

The applicant respectfully requests the following zoning ordinance modifications:

Pursuant to Section 3.4.13, Compliance with Use Standards, a modification by the Town Council of Section 9.3.24.F, Automobile Service Station Canopy Height.

Zoning Ordinance Section to be Modified: Section 9.3.24 Use Standards – Service Stations F. Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".

Proposed Modification:

Canopy Height as measured from the average finished grade to the lowest point on the canopy fascia should not exceed 17'0". The overall height of the canopy shall not exceed 20'6".

Applicant's Justification:

The applicant has consulted with a gas station operator who recently constructed a service station in Ashburn and who likely will be the operator of this gas station. The proposed height is derived from adding a safety factor to the maximum legal truck height of 13'6". This safety factor is needed due to the proposed grading for the property and also due to the possibility of vehicles that might exceed the maximum height restriction. The site slopes downward to the east, which, based on preliminary grading, will be a difference of up to 3'6". The canopy will need a greater height at the lower end in order to remain level. Having an appropriate canopy height with adequate clearances is necessary as a safety factor in the operation of the service station.



LEESBURG BOARD OF ARCHITECTURAL REVIEW MEMO
FROM REGULAR MEETING ON 19 NOVEMBER 2012

To: Irish Grandfield, Environmental Planner
From: Kim K. Del Rance, LEED AP, Preservation Planner
Date: November 21, 2012
RE: Oaklawn Service Station on Land Bay C, Trimble Plaza and Miller Drive (PEC, H-2)

Preservation Planner comments:

This site is bounded by created boundary lines within a planned employment community at the intersection of Miller Drive, a public right of way and private roadways within the development. The proposed building architecture and materials are not compatible with the previously approved neighboring buildings within the development as was agreed to in proffers in ZM#159 and TLZM-2005-0002.

Areas in conflict with the agreed to H-2 Corridor Design Guidelines include, but are not limited to the siting of the buildings, the materials, the massing and height, pedestrian access and screening.

- ◆ This service station has been preceded by approvals for residential design (TLHP-2006-0020) and commercial designs (TLHP-2007-0009 Exxon and TLHP-2006-0112 Winwood Daycare) most of which have gabled roofs on brick structures with cornice detailing and substantial materials which were approved as being in character with historic Leesburg.
- ◆ This convenience store and car wash design shows an excessive amount of an insubstantial material, EIFS, with concrete block and brick- three textured materials and a flat roof with an exaggerated front parapet for signage. Each of these individually would be a candidate for change to be appropriate, but taken together the combination is not in accordance with the guidelines stated above.

P.14 Siting / Relationship to Roadway

*A publicly oriented building, such as an office or commercial facility, should have as its primary orientation the major roadway from which it is accessible. This orientation should be expressed not only in the building's predominant architectural expression but also through the presence of a clearly recognizable entrance. When the approach to such a building is from a different direction than that of the roadway, the **building should have an entrance which is oriented to the direction of approach**, while maintaining a consistent architectural identity that is also recognizable from the adjacent roadway.*

- ◆ The entrance from the main roadway (Miller Drive) is to an entrance from a private road on the west and east to the main convenience store building. The building is sited facing Miller Drive but is obscured by the canopy and gas pumps. Rotating the building to face the intersection and driveway entrance and away from the gas pumps is appropriate.

The side or rear façade of a building located adjacent to and visible from a roadway should be designed with as much attention as the primary façade, even if the roadway is not the building's primary orientation or point of entry.

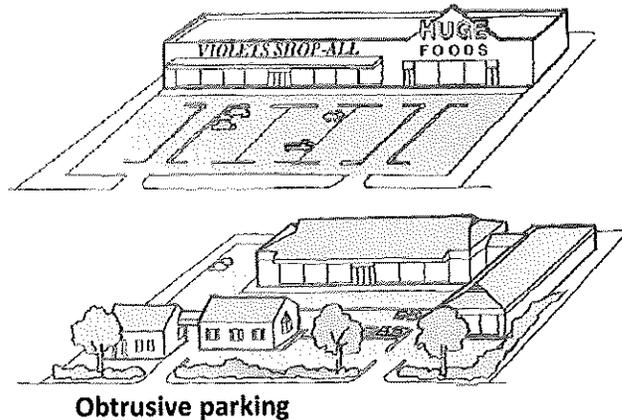
Approaching from the south is essentially facing the rear of the building, the car wash entrance and the dumpster enclosure. The sides of the convenience store are also blank and have no human scale detailing or screening. The car wash entrance is prominent as those entering to the west side will drive towards it as they pass on the west side to find the site entrance.

- ◆ The current siting demands that all facades have more of a presence or are screened appropriately.

P.18 Massing and Roof form

Projects containing many buildings or accommodating a variety of different functions generally should provide variety in building size and massing. A transition from small or low buildings on street frontages to larger and taller structures on the interior of the properties is generally encouraged.

An obtrusive expanse of parking occurs when a single large building mass is located at the rear of the site (below, left). Variety in building massing and siting promotes visual interest and reduces the visual impact of parking (below, right).



Elements such as roofs or canopies should not appear to be pasted on, but should be Three dimensional structural building elements such as ... parapet walls...and roofs should be integral parts of the building design and built of materials similar to or compatible with the rest of the structure.

- ◆ Façade appears to be “pasted on” the front and can be seen easily from the rear as not being functional.
- ◆ As can be seen by drawings of façade and gas pump canopies above, the façade will rise above the height of the canopies which will block its visibility and change the scale of the building in relation to the canopies which are already being requested to be higher than is normally allowed due to the slope of the site.

P.21 Materials and Detailing

The materials used in the construction of a building design can have significant implications for its appearance. Since the solid, exterior wall surface, for example, often accounts for up to 80% of a building's façade area, the selection of an appropriate wall material may be the single most important material choice. Yet, too often materials are selected that have no relationship either

to those found in the Leesburg area or with other materials chosen for the building itself. The following guidelines provide assistance in the selection, application and detailing of appropriate building materials:

- ◆ Artificial veneer materials such as Styrofoam-based simulated stucco, cast or fiberglass stone or brick and plastic appear insubstantial and unauthentic, and should be avoided on new buildings.
- ◆ Traditional roofing- materials that are also appropriate for new construction include slate, wood shakes, and standing seam metal such as copper, tin or aluminum.
- ◆ The history and nature of Leesburg indicate that wood, red brick and native stone are prevalent building materials; these materials likewise are appropriate for newer construction throughout town. Stucco+ and concrete masonry occur less frequently, but may be acceptable building materials in some instances, depending on a building's style and function.
- ◆ The specific materials used on a building or set of related buildings should be compatible with each other in terms of size, shape and texture. While variety in the size or shape of different building components may add interest to a building's exterior appearance, more than one highly textured material such as stone, ashlar-faced concrete block or wood shingled roofs should not be used on a single building. Instead, such materials should be combined with those that are simpler and less textured to avoid a confusing or overly elaborate appearance.
- ◆ While the selection of materials for new construction should be influenced by those materials that are familiar in the built environment of Leesburg, the architectural detailing of such materials-the way in which they are used in relation to other materials in the building- may be less traditional as long as it relates to the overall design of the building. The use of brick, split-faced concrete block and stucco or EIFS (which is used is unclear) all on this small building is too elaborate and the variety should be reduced to two or fewer of these materials.
- ◆ While this is only a second submission review and colors are not mentioned, P. 23 should be referred to by the applicant and designers before submitting an application for a Certificate of Appropriateness.

P. 25 Signage, P. 28 Screening, P.32 Site Access are all applicable to specific elements of this proposal

To minimize traffic conflicts, vehicular entrances and exits to a site should be consolidated at a single location a sufficient distance away from street intersections with high traffic volume.

- ◆ The main entrance follows this, but the two additional entrances on the east side of the site should be minimized.

The apparent width of entrances and driveways should be minimized wherever possible by the provision of a planted median strip of at least 6' in width between incoming and outgoing traffic particularly if two or more lanes are provided in each direction.

- ◆ All three entrances are wide and currently there is no planted median, only stripes which do not protect the pedestrians from oncoming traffic in either direction.

Entrances and driveways should permit safe and convenient pedestrian crossing where they intersect sidewalks and other pedestrian circulation systems.

- ◆ The current pedestrian access from across Trimble Plaza is awkward, long and wraps around the rear of the building requiring pedestrians to cross in front of the car wash entrance driveway as traffic leaves the roadway.

*Pedestrian access to sites should be provided from **all adjacent public sidewalks**, protected from vehicular traffic by grade separation, plant materials and/or berming.*

- ◆ Currently there are two one pedestrian accesses which have none of the above conditions.

Where appropriate, effort should be made to provide secondary pedestrian access to sites and linkages between adjacent sites by the provision of pedestrian paths not necessarily associated with public roadways.

- ◆ This site is a candidate for having additional pedestrian access since it is completely surrounded by roadways.

P.33 Parking Lots

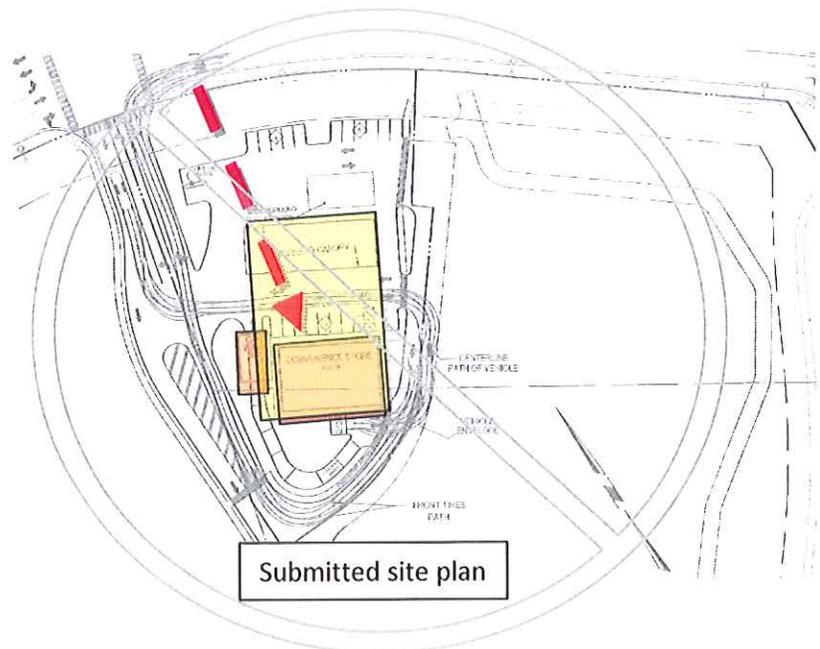
... it is the provision of on-site parking that most distinguishes the character of new and recent development

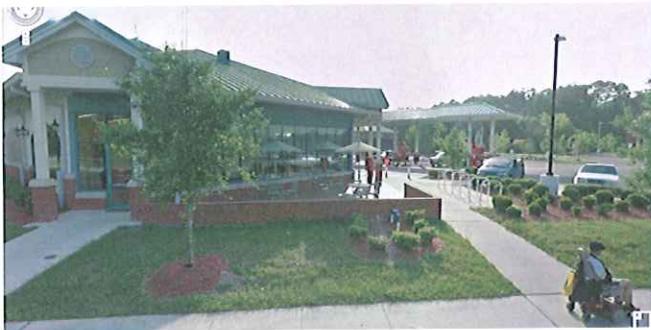
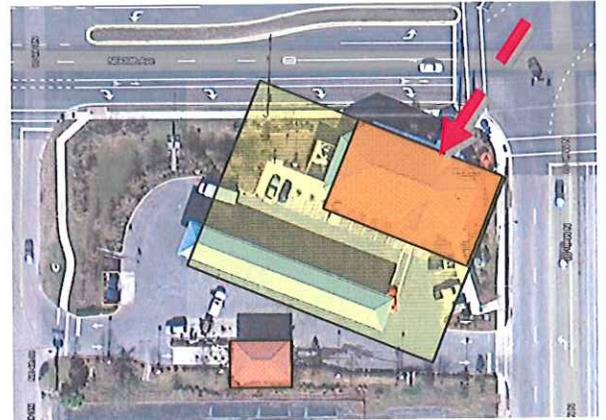
*A single large expanse of parking should be avoided. Instead, parking should be provided in smaller, well-defined areas, separated and screened by features such as berms, access drives, landscaping and/or buildings. Parking areas **should not** be located exclusively at the front of building sites.*

- ◆ Parking for this site is exclusively at the front of the site, which is **not to be done**.

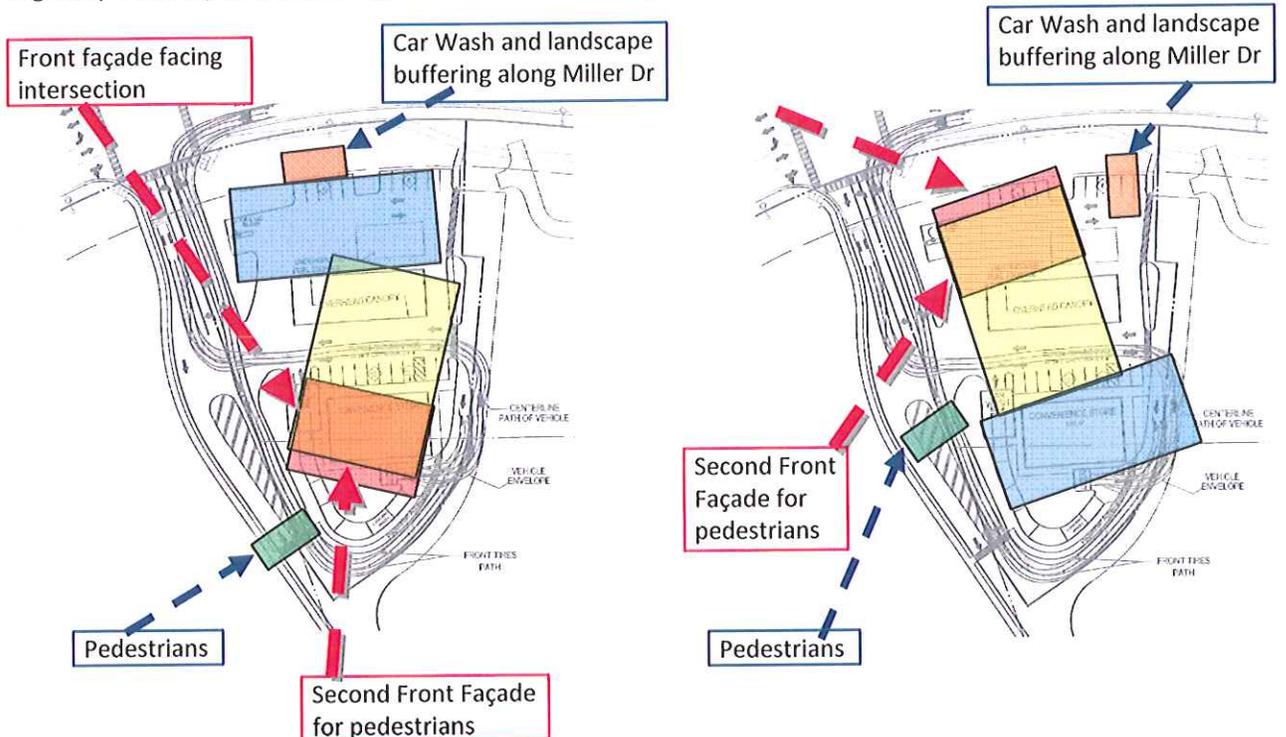
Wherever possible, some of the required parking should be provided at the sides or rear of the site, following the traditional pattern of downtown Leesburg. Promoting variety in the placement of buildings on the site (see Size and Scale, Chapter I/Building Design Standards) may also help achieve spatial variety in the placement of parking areas.

- ◆ This site should have alternative parking placement using the rear and/or sides of the building for smaller parking areas.





Above are two examples of a gas station, convenience store and car wash sited along two public roadways where the main building is facing the main intersection and has a pedestrian approach while maintaining views of the gas pumps from inside the store, allowing free movement through the parking lot for the store, the pumps and the car wash. The hipped roofs shown here are more appropriate than the flat roof with pasted on front façade which is more suited to a highway rest stop or a shopping center not in Leesburg, VA.



Above left is one example of a slight siting change by moving the car wash up to inside the berm and landscape buffer with proper screening and allowing safe pedestrian crossing at the SW corner that would meet the H-2 guidelines, but it is unknown if this would also meet all other site requirements.

Above right is an example of siting change that would meet the H-2 guidelines, but this is not possible as the site is currently bounded.

STAFF COMMENTS SUMMARY

There are alternative site placements that are appropriate to this site that will follow the agreed upon H-2 Corridor Guidelines. It is unclear at this point if the site boundaries as drawn can support an appropriately sited building, canopy and car wash. Other items such as size, scale, massing and height can be adjusted later, but building siting should be addressed at this stage. If new siting that meets the H-2 guidelines cannot be met with these boundaries, then staff recommends further site plan research and the appropriate site boundaries be established before approval of this use is considered.

BOARD OF ARCHITECTURAL REVIEW COMMENTS SUMMARY FROM NOVEMBER 19, 2012 BUSINESS MEETING

Several members of the board expressed issues with the building architecture. They agreed with staff that the architecture can be addressed at a later date and the security need for the gas pumps to be visible from the store is valid, but the site issues should be addressed by looking at alternative site arrangements and that a better site plan should be used.

Kim K. Del Rance, LEED AP
Preservation Planner



Department of Planning & Zoning

25 West Market Street ■ 20176 ■ 703-771-2765 ■ Fax: 703-771-2724 ■ www.leesburgva.gov

June 14, 2012

Oaklawn Development Partners, LLC
Attn: Mr. Andy Shuckra
P.O. Box 1573
Ashburn, VA 20146

RE: Oaklawn Gas Station, Landbay C - Waiver Request for Traffic Study
Location: Trimble Plaza, SE
PIN# : 233-20-0550, 233-29-6350 and 233-29-9822
Related Applications: ZM #159, TLZM 2005-0002, TLSE 2012-0005

Dear Mr. Shuckra:

Staff received your special exception application for a gas station proposed to be located in Landbay C along with your request, dated April 6, 2012, for a waiver of a traffic study. The following is my response to your request.

Ordinance Requirements. TLZO Sec. 3.4.6.F.3. **Submittal Requirements, Written statement, Traffic Impact Analysis** specifies that a traffic impact analysis be submitted with special exception applications. This analysis will include the maximum expected trip generation and the trip distribution by mode and time of day based on the current Institute of Transportation Engineers (ITE) Manual, the internal road network, and the connection into the existing transportation network. A traffic study is required if:

- The property is adjacent to a road that carries, or proposes to carry, more than 550 vehicle trips per day (VTD); or
- The property contains a road proposed to carry more than 500 VTD; or
- The property is adjacent to a road that currently or is proposed to carry 100 or more peak hour trips; or
- If the traffic study for the subject property is more than one (1) year old.

Your special exception application would trigger all four of the criteria listed above.

Waiver and Modification Standards. TLZO Sec. 3.4.6. F.3 (e). provides for the option to waive the aforementioned requirements. These standards state in pertinent part that the requirements may be waived at the discretion of the Land Development Official in response to a written request from the applicant when filed with the initial application. Waivers may be granted upon finding that a traffic analysis is unnecessary due to:

TLSE 2012-0005, Oaklawn Gas Station
Planning Commission Public Hearing Staff Report
January 17, 2013
Page 31

Oaklawn Special Exception for Gas Station – Landbay C
Response to Request for Waiver of Traffic Study
6/14/2012
Page 2 of 2

- existence of previous studies and analysis which adequately cover the extent of the proposed development and impacts; or
- receipt of adequate proof from the applicant that the proposed use does not increase the number of employees or customers using the site.

Analysis and Response. Staff notes that traffic counts had been done within the past year at the intersection of Miller Drive and Brown Roan Drive. In a memo received from Michael Armstrong, Engineer with the Department of Public Works, dated April 24, 2012 (Attachment 1) it was noted that review of these counts and consideration of the proposed development showed that the subject intersection would meet acceptable levels of service with the service station/car wash/convenience store uses. Therefore, the waiver requirement for the existence of a previous study which adequately covers the extent of the proposed development (a service station/car wash/convenience store) has been met. As such, in the opinion of the Transportation Division, the traffic study could be waived based on the following two conditions:

1. Future land development within Landbay C will impact this intersection and that as a result the Town will require a traffic study for future development proposals beyond the subject service station/car wash/convenience store within Landbay C.
2. As a part of this special exception application review, the applicant must demonstrate how WB-50 and SU vehicles will adequately access the site.

Based on the above findings and conditions, the Zoning Ordinance requirements in Section 3.4.6.F 3 pertaining to submittal of a traffic study with this special exception application are hereby waived.

If you have any questions about the contents of this letter please contact me at 703-771-2770 or via email at sberryhill@leesburgva.gov.

Sincerely,



Susan Berry Hill, AICP
Director Planning and Zoning
Town of Leesburg

Attachment: Memo from Michael Armstrong, Department of Public Works

cc: Mike Watkins, Senior Planner, DPZ
Michael Armstrong, Engineer, DPW

- 3.(10) (Plat Content) Label all lines on all sheets. For example, provide a label for the 100-yr. overland relief path on Sheet 1. Also label all existing easements and provide recordation references. (Sht. 1-5) Z.O. 3.4.6.E.20. *Existing contours are not shown on Sheet 4 and the proposed contours are not labeled. Provide this information with the next submission. (Sht. 4) Z.O. 3.4.6.E.20.*

Storm Drainage:

- 4.(12) (BMP Requirements) With regard to Note 17, provide some details of a proposed strategy and conceptual layout including preliminary calculations for meeting the requirements of a BMP "Hot Spot". (Sht. 1) DCSM 10-120.2G(8). *At a minimum, provide the following narratives and notes regarding "Hot Spot" compliance on the Special Exception Plat:*

BMP Hot Spot Narrative

This site will operate as a fuel storage and dispensing station and is considered a Hot Spot per DCSM (5-640.4J). A Baysaver or equivalent approved product will be required to contain minor spills on site. The designed system will provide a primary separation manhole, secondary storage manhole (or facility) and Baysaver separator (or approved equivalent). A shutoff valve, easily accessible and signed, is to be installed on the downstream side of Baysaver or equivalent. In the event of spill, the valve is to be closed and the spill contained within the pipes and structures which will be over-sized to hold the contaminated rainwater and material until HAZMAT teams reach the site. Secondary storage and/or storage facility shall be designed to contain a minimum of 2500 gallons.

Notes:

1. BMP Hot Spot structure details to be provided on Final Site Plan.
 2. BMP Hot Spot Maintenance Plan to be provided on Final Site Plan.
 3. BMP Hot Spot site specific Narrative to be provided on Final Site Plan.
 4. BMP Hot Spot structure sizing computations to be provided on Final Site Plan.
 5. BMP Hot Spot structure capacities to be provided on Final Site Plan.
 6. The Applicant shall provide evidence that an approved Emergency Response Plan has been filed with and approved by the Town as well as the Loudoun County Department of Fire and Rescue Services.
- 5.(13) (Storm Sewer Configuration) The storm sewer layout shown does not seem to take into consideration the future development of adjoining lots and does not provide curb inlets at intersections and entrances. Revise the storm sewer layout to include the location of curb inlets, provide 90° crossings of roads, utilities and access ways and minimizing the number of manhole structures in the paved areas of the access ways, while still providing points of connection for BMP facilities and future development in order to demonstrate that this can be accomplished without modifications. (Sht. 5) Z.O. 3.4.6.E.23. *The storm sewer layout should be revised (simplified) further by extending the trunk line to the inlet closest to Miller Drive before crossing the street, thus providing a usable landscaped area just south of the proposed "air and vac" island. (Sht. 2-5) Z.O. 3.4.6.E.23.*

Transportation/Vehicular Access:

- 6.(22) (Traffic Circulation) The right/left turn lanes at the western side of Trimble Way does not appear to function properly in the interim (where the proposed gas station facility is the only structure located on this landbay) or ultimate conditions, however, based upon the proximity of the site entrance (being within 200') to the public right of way, a complete, updated traffic study will be required prior to any future development in Land Bay C. *There are similarities between the proposed major access point for this gas station site (proposed adjacent to Miller Drive) and the existing gas station (Sheetz, located adjacent to Edwards Ferry Road). In*

both of these examples, a two way private travelway, with a single lane in each direction and a very minimal left turn lane into the site, serves or will serve the respective gas stations. In an effort to avoid another "Sheets" condition with this project (i.e. excessive stacking in the public right of way, it is recommended that the Applicant provide two lanes into the site from Miller Drive (onto Trimble Plaza) with the innermost lane dropping off as a left turn only lane into the proposed gas station. The outer lane would then enhance internal vehicular circulation by serving as a bypass lane for access to the rest of the Landbay which would minimize turning movement conflicts that potentially could cause excessive stacking in the Miller Drive public right of way. Z.O. 3.4.12.D.

- 7.(27) (Roadway Network Configuration) The tanker truck turning movement analysis appears to have some conflicts. At the entrance into the site from Trimble Plaza, the truck swings into the left lane in order to make a right turn and appears to clip the curb on the right hand side. Also the turn from access way at station onto Trimble Plaza appears to clip the curb. Review and revise to provide for safe maneuvering of the design vehicle. (Sht. 2) Z.O. 3.4.6.E.7. *The tanker truck turning movement analysis appears to still have some conflicts. When the truck exits Trimble Plaza onto Battlefield Parkway, the truck swings into the left lane in order to make a right turn and creates an unsafe condition. Recommend revising access road around the gas station itself, to accommodate WB-50 vehicles if there are cars waiting to turn at intersections, it does not create an unsafe condition. Review and revise to provide for safe maneuvering of the design vehicle. (Sht. 2) Z.O. 3.4.6.E.7.*

Landscaping:

- 8.(31) (Screening between Uses) Sheet 1, Note 13, fourth sentence states: "The other boundaries are part of commercial Land Bay C under the ZM#159 concept plan in which all uses are intended to interrelate with one another without a need to be screened and buffered from each other, and since the screening and buffering is being provided around the perimeter of the land bay adjacent to public roads under the proffered landscape plan for ZM#159".

The Rezoning Concept Plan/proffers did not include this language.

Revise the statement (note) regarding perimeter parking lot screening in the upper left hand corner of Sheet 4 to read: "The perimeter parking lot screening adjacent to public streets is not required as an S-2 or S-3 screen has been provided adjacent to the public streets around the entire perimeter of Land Bay C per the Oaklawn at Stratford Rezoning ZM-159 and the Oaklawn Commercial Preliminary Subdivision Plat TLPS-2004-0003." Z.O. 12.5.1.

Provide a continuous landscape area at least five (5) feet in width between the parking lot and the property line. See attachment showing areas where perimeter parking lot screening is required. (Sheet 4) Z.O. 12.5.4.A.

George Clausen, LSIT
Senior DPR Engineer

John D. Johnston, LA
Senior DPR Planner



The Town of Leesburg
DEPARTMENT OF PLANNING AND ZONING

TO: Irish Grandfield
Project Manager

DATE: November 19, 2012

FROM: Kevin Lawlor
Sr. Planner – Zoning

RE: TLSE-2012-0005
2nd Submission

I have reviewed the proposed Oaklawn Landbay C gas station/convenience store/carwash, engineer date 10/12/12, received date 10/15/12, prepared by Paciulli Simmons & Associates. The applicant proposes to build a gas station/convenience store/carwash (4365 s.f.) in Landbay C.

I provide the following information in review of the request.

1. B.A.R. approval is required for this project in accordance with the proffers from TLZM-1999-0159.

Kevin P. Lawlor



The Town of Leesburg
INTEROFFICE MEMORANDUM
DEPARTMENT OF PUBLIC WORKS

TO: Irish Grandfield, Environmental Planner
FROM: Michael P. Armstrong, EIT, LSIT, Engineer
DATE: November 19, 2012
RE: Oaklawn – Gas Station, Convenience Store, and Car Wash

The following comments are offered by the Department of Public Works Transportation Division to be incorporated into the Department of Planning & Zoning comments to be addressed by the applicant. Please advise the applicant to revise the plans in accordance with the comments below and resubmit.

1. Due to the anticipated 95th percentile queue, it is recommended that a “Do Not Block Intersection” sign be installed for northbound traffic on Trimble Way prior to the main entrance to the gas station.

Michael P. Armstrong, EIT, LSIT
Engineer

CC: Calvin K. Grow, P.E., Transportation Engineer



The Town of Leesburg
INTEROFFICE MEMORANDUM
DEPARTMENT OF UTILITIES

To: James P. ("Irish") Grandfield, AICP, Senior Planner

From: *AE* Aref Etemadi, Deputy Director of Utilities

Date: November 1, 2012

Subject: Oaklawn Landbay C, Gas Station
TLSE – 2012-0005
2nd Submission

We offer the following comments:

1. Due to the angle of sewer penetration at manhole "a" the better solution will be to make this connection at manhole "S22" (previous comment#5) unless the lateral enters the manhole at bench.
2. Show the water service to the store (previous comment#6).
3. The angle of the crossing of 66" storm and the waterlines is unacceptable unless the water is above the storm (previous comment#7).
4. If food preparation is proposed within the store a grease trap or interceptor will be required and must be shown or added as a note to the plans (previous comment#8).
5. Is a lawn irrigation system proposed for this use?
6. Cleanouts are required 5' from the front of the buildings on both lateral sewers exiting the car wash and the store.
7. See attached requests for changes to utility layout.
8. Please respond to previous comments 9-12.

PRESENTED: February 26, 2013

RESOLUTION NO. _____

ADOPTED: _____

A RESOLUTION: APPROVING TLSE-2012-0005, OAKLAWN GAS STATION, A SERVICE STATION, CONVENIENCE STORE, AND CAR WASH LOCATED IN OAKLAWN LAND BAY C ON PROPERTY SHOWN ON THE SPECIAL EXCEPTION PLAT AND IDENTIFIED AS PORTIONS OF THE FOLLWING THREE LOTS: LOUDOUN COUNTY PARCEL INDENTIFICATION NUMBERS (PIN) 233-20-0550, 233-29-6350, 233-29-9822

WHEREAS, Oaklawn Development Partners LLC is requesting Special Exception approval for a service station, convenience store and car wash in the PEC (Planned Employment Center) zoning district on 1.3 acres in Land Bay C of the Oaklawn site in south Leesburg on property identified as Loudoun County Parcel Identification (PIN) numbers 233-20-0550, 233-29-6350, 233-29-9822; and

WHEREAS, the Planning Commission held a duly advertised Public Hearing on January 17, 2013; and

WHEREAS, the Town Council held a duly advertised Public Hearing on February 26, 2013; and

WHEREAS, Town Council has determined the proposed use meets the special exception use standards for a service station, convenience store, and car wash set forth in the Zoning Ordinance; and

WHEREAS, Town Council has determined that the Approval Criteria of Zoning Ordinance Section 3.4.12 have been satisfied; and

WHEREAS, Town Council has determined that the proposal would serve the public necessity, convenience, general welfare and good zoning practice.

A RESOLUTION: APPROVING TLSE-2012-0005, OAKLAWN GAS STATION, A SERVICE STATION, CONVENIENCE STORE, AND CAR WASH LOCATED IN OAKLAWN LAND BAY C ON PROPERTY SHOWN ON THE SPECIAL EXCEPTION PLAT AND IDENTIFIED AS PORTIONS OF THE FOLLOWING THREE LOTS: LOUDOUN COUNTY PARCEL IDENTIFICATION NUMBERS (PIN) 233-20-0550, 233-29-6350, 233-29-9822

THEREFORE, RESOLVED by the Council of the Town of Leesburg in Virginia that Special Exception application TLSE-2012-0005, Oaklawn Gas Station, is hereby approved to permit a service station, convenience store, and car wash on the property identified as portions of Loudoun County Parcel Identification (PIN) numbers 233-20-0550, 233-29-6350, 233-29-9822, subject to the following conditions:

1. Substantial Conformance. This special exception shall be in substantial conformance with the special exception plat entitled "Oaklawn-Commercial Land Bay C", Sheets 1-8, revised through 12-31-2012 and prepared by Paciulli Simmons & Associates.
2. No Waivers Expressed or Implied. With the exception of the modification listed below, approval of this special exception does not express or imply any waiver or modification of the requirements set forth in the Subdivision and Land Development Regulations, the Zoning Ordinance, or the Design and Construction Standards Manual, except as may have been approved as part of this application. Final plats, site plans, and construction drawings are subject to applicable town regulations.
3. Modification Granted. Modification to TLZO 9.3.24F (Canopy Height) is hereby granted as follows:

"Canopy height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0" except as necessary to allow the canopy to remain level where the finished grade of the land slopes beneath the canopy. The final grade on the land beneath the canopy shall not slope more than a total of three and one-half (3.5) vertical feet. The height of the canopy shall not exceed 3'3" above the lower edge of the canopy fascia at any one point and the maximum height of the canopy at any one point shall not exceed 20'9."
4. Mechanical Equipment. All mechanical equipment, including roof-top equipment, shall be screened from public view by appropriate architectural elements.
5. Building Elevations. The structures shall be approved by the Board of Architectural Review (BAR) in accordance with Proffer #V.20 of #ZM-159.

A RESOLUTION: APPROVING TLSE-2012-0005, OAKLAWN GAS STATION, A SERVICE STATION, CONVENIENCE STORE, AND CAR WASH LOCATED IN OAKLAWN LAND BAY C ON PROPERTY SHOWN ON THE SPECIAL EXCEPTION PLAT AND IDENTIFIED AS PORTIONS OF THE FOLLWING THREE LOTS: LOUDOUN COUNTY PARCEL INDENTIFICATION NUMBERS (PIN) 233-20-0550, 233-29-6350, 233-29-9822

6. Building Facades. All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, coloring and detailing.
7. Traffic Signage The applicant shall install a “Do Not Block Intersection” sign for northbound traffic on Trimble Way prior to the main entrance to the gas station.
8. Car Wash Façade. Provide one or more windows on the building elevation of the car wash facing Trimble Way to better define the streetscape and break up the mass of the building.

PASSED this 26th day of February, 2013

Kristen C. Umstattd, Mayor
Town of Leesburg

ATTEST:

Clerk of Council