



**LEESBURG BOARD OF ARCHITECTURAL REVIEW
STAFF REPORT**

**WORK SESSION: 5 NOVEMBER 2012
AGENDA ITEM 6a**

BAR Case No. TLZM-2012-0005: New construction of a service station, convenience store and car wash in a planned development.

Reviewer: Kim K. Del Rance, LEED AP
Address: Land Bay C, Trimble Plaza and Miller Drive
Zoning: PEC, H-2 Overlay District
Referral Request: Irish Grandfield, Environmental Planner
Owner Representative: Christine Gleckner, AICP, Walsh, Colucci, Lubely, Emrich & Walsh PC

APPLICABLE GUIDELINES: H-2 CORRIDOR BY PROFFER

BUILDING DESIGN GUIDELINES

p.12 Design Expression

For new commercial centers or residential developments incorporating multiple buildings, each individual building or structure should be compatible with other buildings within the overall design plan. Even if developed over time, there should be a master design plan that relates buildings to each other and to the total development.

- ◆ This service station has been preceded by approvals for residential design (TLHP-2006-0020) and commercial designs (TLHP-2007-0009 Exxon and TLHP-2006-0112 Winwood Daycare) most of which have gabled roofs on brick structures with cornice detailing and substantial materials which were approved as being in character with historic Leesburg.
- ◆ This convenience store and car wash design shows an excessive amount of an insubstantial material, EIFS, with concrete block and brick- three textured materials and a flat roof with an exaggerated front parapet for signage. Each of these individually would be a candidate for change to be appropriate, but taken together the combination is not in accordance with the guidelines stated above.

Examples of buildings approved in Oaklawn to date:



Winwood Children's Daycare Center



Northwest Federal Credit Union



Pulte Homes at Oaklawn

P.14 Siting / Relationship to Roadway

A publicly oriented building, such as an office or commercial facility, should have as its primary orientation the major roadway from which it is accessible. This orientation should be expressed not only in the building's predominant architectural expression but also through the presence of a clearly recognizable entrance. When the approach to such a building is from a different direction than that of the roadway, the building should have an entrance which is oriented to the direction of approach, while maintaining a consistent architectural identity that is also recognizable from the adjacent roadway.

The side or rear façade of a building located adjacent to and visible from a roadway should be designed with as much attention as the primary façade, even if the roadway is not the building's primary orientation or point of entry. Windows, doors, balconies and varied rooflines all help to add visual interest. When the buildings must present blank side or walls to the roadway, these must be screened from view with appropriate plant materials, berms or other screening.

- ◆ The entrance from the main roadway (Miller Drive) is to an entrance from a private road on the west and east to the main convenience store building. If you approach from the south and split to go one way or another around the building to enter the parking lot you are facing essentially the rear of the building, the car wash entrance and the dumpster enclosure.
- ◆ The back of the convenience store building is what all entering from the south will see, which should be altered to fit the approaches. The sides of the convenience store are also blank and have no human scale detailing or screening. The car wash entrance is prominent as those entering to the west side will drive towards it as they pass on the west side to find the site entrance. The siting of the car wash demands it has more of a presence or be screened appropriately.

*P.16 Features that **Promote** Human Scale Buildings (to be encouraged– 3 of the 7 listed apply):*

1. *Balconies, columns, covered walkways or other façade projections or recesses*
The hanging canopy over the entrance is appropriate and encouraged.
2. *Walls with doors and windows, differentiated wall surfaces and other building details*
The front façade wall treatment with various windows and changing materials from brick to what appears to be split faced concrete block is encouraged.
3. *Textured and/or modular building materials (brick, clapboards)*
The brick and split-faced block used in harmony is appropriate and encouraged

*P.16 Features that **Obscure or Destroy** human scale (to be avoided – 3 of the 7 listed apply):*

1. *Blank walls*

The blank walls should be detailed to a human scale or screened.

2. *Smooth or panelized building materials (stucco, metal or prefabricated concrete panels)*

The smooth stucco on the rear and side should be detailed to a human scale or have the materials altered.

3. *Flat (not visible) roofs*

The roofs should be compatible with the existing approved roofs in this development, all of which are gabled with shingles.

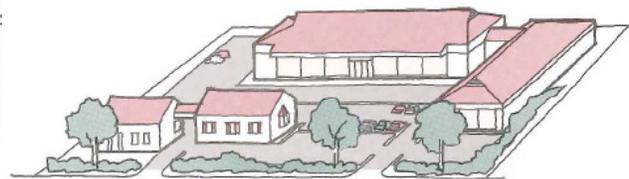
P.18 Massing and Roof form

Projects containing many buildings or accommodating a variety of different functions generally should provide variety in building size and massing. A transition from small or low buildings on street frontages to larger and taller structures on the interior of the properties is generally encouraged.

An obtrusive expanse of parking occurs when a single large building mass is located at the rear of the site (below, left). Variety in building massing and siting promotes visual interest and reduces the visual impact of parking (below, right).

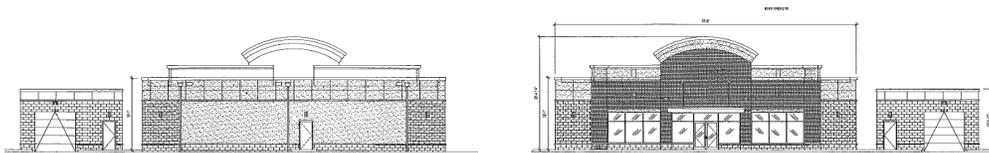


Obtrusive parking

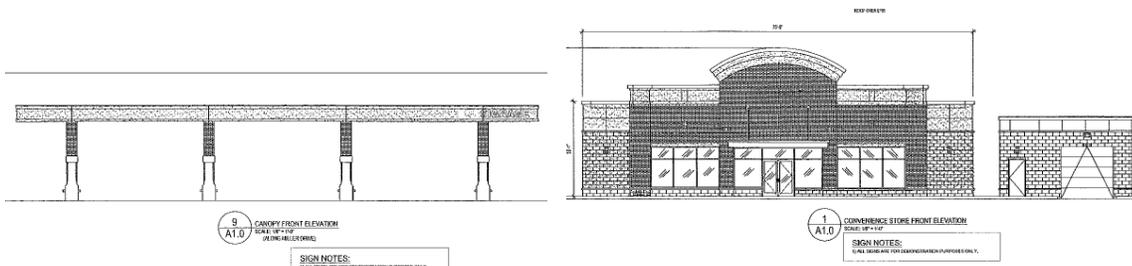


parking impact reduced

Elements such as roofs or canopies should not appear to be pasted on, but should be Three dimensional structural building elements such as ... parapet walls...and roofs should be integral parts of the building design and built of materials similar to or compatible with the rest of the structure.



Façade appears to be pasted on front and can be seen easily from rear as not being functional



As can be seen by drawings of façade and gas pump canopies above, the façade will rise above the height of the canopies which will block its visibility and change the scale of the building in relation to the canopies which are already being requested to be higher than is normally allowed due to the slope of the site.

P.21 Materials and Detailing

The materials used in the construction of a building design can have significant implications for its appearance. Since the solid, exterior wall surface, for example, often accounts for up to 80% of a building's façade area, the selection of an appropriate wall material may be the single most important

material choice. Yet, too often materials are selected that have no relationship either to those found in the Leesburg area or with other materials chosen for the building itself. The following guidelines provide assistance in the selection, application and detailing of appropriate building materials:

- ◆ Artificial veneer materials such as styrofoam-based simulated stucco, cast or fiberglass stone or brick and plastic appear insubstantial and unauthentic, and should be avoided on new buildings.
- ◆ Traditional roofing- materials that are also appropriate for new construction include slate, wood shakes, and standing seam metal such as copper, tin or aluminum.
- ◆ The history and nature of Leesburg indicate that wood, red brick and native stone are prevalent building materials; these materials likewise are appropriate for newer construction throughout town. Stucco+ and concrete masonry occur less frequently, but may be acceptable building materials in some instances, depending on a building's style and function.
- ◆ The specific materials used on a building or set of related buildings should **be compatible with each other** in terms of size, shape and texture. While variety in the size or shape of different building components may add interest to a building's exterior appearance, **more than one highly textured material such as stone, ashlar-faced concrete block or wood shingled roofs should not be used on a single building. Instead, such materials should be combined with those that are simpler and less textured to avoid a confusing or overly elaborate appearance.**
- ◆ While the selection of materials for new construction should be influenced by those materials that are familiar in the built environment of Leesburg, the architectural detailing of such materials-the way in which they are used in relation to other materials in the building-may be less traditional as long as it relates to the overall design of the building.

The use of brick, split-faced concrete block and stucco or EIFS (which is used is unclear) all on this small building is too elaborate and the variety should be reduced to two or fewer of these materials.

While this is only a second submission review and colors are not mentioned, P. 23 should be referred to by the applicant and designers before submitting an application for a Certificate of Appropriateness.

P. 25 Signage

...The Board of Architectural Review will review sign requests to determine appropriate color, design and location within the H-2 corridor.

The location is the only of these items being reviewed. The current location in the northwest corner of the site angled to face the intersection of the public and private roads is appropriate.

P. 28 Screening

Preferred methods of screening building utilities and equipment include berming and/ or vegetative planting, or walls and fences. Plant materials selected for screening should provide a hardy, dense screen throughout the year. Walls and fences should be located in close proximity to and made of materials compatible with those of the adjacent building(s), and should be both solid and opaque. The Design of walls in particular should be integrated with that of the building structure. If they are not of the same materials the building they should be the same color.

The dumpster enclosure details are not given and it is not shown in the elevations so no review can be made of it at this review, but this will be required for a Certificate of Appropriateness.

P.32 Site Access

Providing clear and convenient access to a development site for both motorists and pedestrians is often an early concern in the development process. Yet, when a number of parcels along a roadway frontage

of limited length each take an independent approach to providing site access, complicated or confusing traffic patterns can result and create potential safety hazards. Numerous or excessively wide entrances also can reduce roadside opportunities for berming or the retention of vegetation that enhance the natural character of the area or may provide visual screening. The following guidelines offer suggestions for minimizing the impact of providing convenient site access:

Direct access from the roadway to development sites adjacent to the roadway is discouraged. Access should be provided by secondary roads, streets or common driveways wherever possible.

- ◆ The current main entrance follows this guideline

When site access must be provided directly from the main roadway, especially where existing banks or berms provide edge definition or visual screening along the roadway, the access point should be located where only minimal disturbance of existing topography is necessary.

*To minimize traffic conflicts, **vehicular entrances and exits to a site should be consolidated at a single location** a sufficient distance away from street intersections with high traffic volume.*

- ◆ The main entrance follows this, but the two additional entrances on the east side of the site should be minimized.

*The apparent **width of entrances and driveways should be minimized** wherever possible by the provision of a **planted median strip of at least 6' in width between incoming and outgoing traffic** particularly if two or more lanes are provided in each direction.*

- ◆ All three entrances are wide and currently there is no planted median, only stripes which do not protect the pedestrians from oncoming traffic in either direction.

*Entrances and driveways should permit **safe and convenient pedestrian crossing** where they intersect sidewalks and other pedestrian circulation systems.*

- ◆ The current pedestrian access from across Trimble Plaza is awkward, long and wraps around the rear of the building requiring pedestrians to cross in front of the car wash entrance driveway as traffic leaves the roadway.

*Pedestrian access to sites should be provided from **all adjacent public sidewalks**, protected from vehicular traffic by grade separation, plant materials and/or berming.*

- ◆ Currently there are two one pedestrian accesses which have none of the above conditions.

Where appropriate, effort should be made to provide secondary pedestrian access to sites and linkages between adjacent sites by the provision of pedestrian paths not necessarily associated with public roadways.

- ◆ This site is a candidate for having additional pedestrian access since it is completely surrounded by roadways.

P.33 Parking Lots

... it is the provision of on-site parking that most distinguishes the character of new and recent development

*A single large expanse of parking should be avoided. Instead, parking should be provided in smaller, well-defined areas, separated and screened by features such as berms, access drives, landscaping and/or buildings. Parking areas **should not** be located exclusively at the front of building sites.*

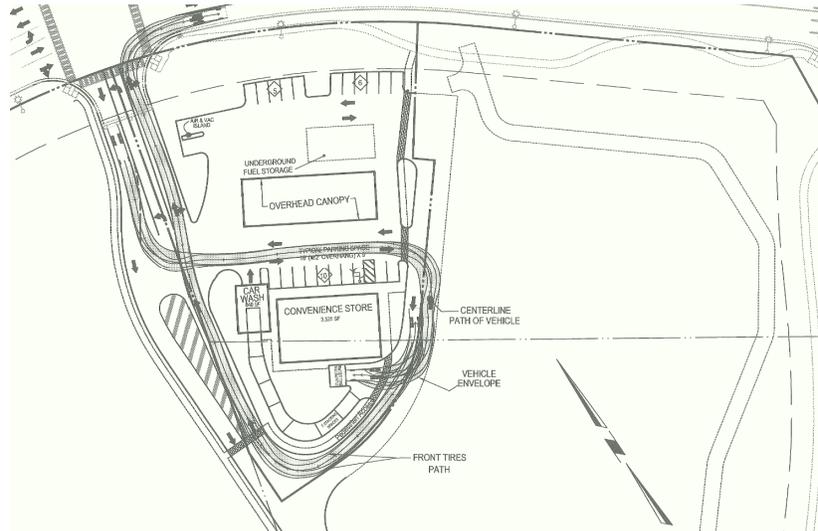
- ◆ Parking for this site is exclusively at the front of the site, which **is not to be done**.

Wherever possible, some of the required parking should be provided at the sides or rear of the site, following the traditional pattern of downtown Leesburg. Promoting variety in the placement of buildings on the site (see *Size and Scale, Chapter I/Building Design Standards*) may also help achieve spatial variety in the placement of parking areas.

- ◆ This site should have alternative parking placement using the rear and/or sides of the building for smaller parking areas.

STAFF SUMMARY

This report covers more than the minimum requirements for a rezoning referral so below it is broken into the main areas needed to review a referral. They are **Size, scale, massing, height and siting**.



The **size** of the buildings is appropriate in general, but the relation of the size of one to another is affected by scale, roof and façade treatments, so it cannot be considered alone.

The **scale** of the buildings and canopies are awkward due to the height of the false façade parapet being higher than the canopies over the gas pumps. Since the reason for the rezoning includes raising the height of the canopies above what is allowed, the height of the front façade should not be raised even higher as it is a one story building. Visibility of the rear of the parapet from behind the building makes it obvious it is only decorative and not functional, which is not traditional to Leesburg.

The **massing** of the buildings is not inappropriate, but the relationship between the buildings can be more appropriate with regards to the overall site and access issues.

The **height** of the convenience store front façade parapet wall is awkward, not functional and appears to be pasted on, which is not typical of Leesburg.

The **siting** on this parcel has the main building placed at the rear of the lot facing the major roadway (Miller Drive) and the parking lot is in front of it, these two things are in the guidelines, on p.18 and on p.33 it states *Parking areas should not be located exclusively at the front of building sites.*

In addition to the positioning of the building on the site is the fact that the rear and sides of the building have not been treated as importantly as the front, which is required when a building faces more than one roadway. Pedestrian access and driveway entrances are also part of the siting and do not fit within the guidelines as stated above.

Overall, staff finds that this proposal does not meet the H-2 Corridor Design Guidelines without a major redesign. There may be alternative building and parking placements that would better follow the guidelines and should be explored before rezoning this parcel.

PROJECT:
**SCHMITZ
OAKLAWN**

OAKLAWN COMMERCIAL
LANDBAY C
TOWN OF LEESBURG

Client / Owner Name:
Schmitz Services
Attn: Eric Schmitz
8526 Leesburg Pike
Vienna, VA 22182

ISSUED	
D.A.R. SUBMISSION	10-16-12
NO. REVISION	DATE

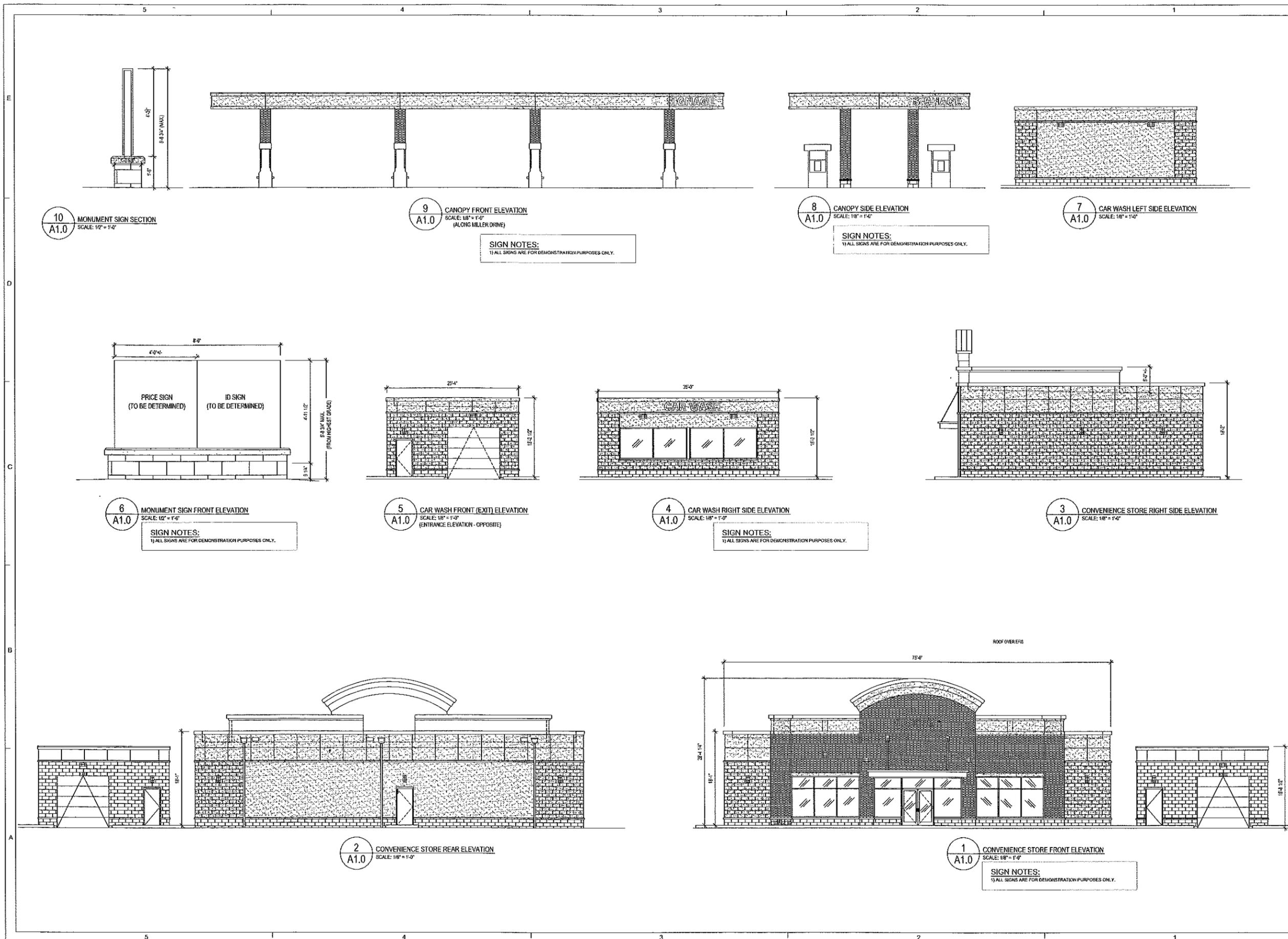
DESIGNED/DRAWN BY: BJ
REVIEWED/APPROVED BY: BJ
FILE NO:
SCALE:

THIS DOCUMENT AND THE IDEAS AND DESIGNS INCORPORATED HEREIN AS AN INSTRUMENT OF PROFESSIONAL SERVICE, IS THE PROPERTY OF BJ DESIGN, LLC AND IS NOT TO BE COPIED OR USED, IN WHOLE OR IN PART FOR ANY OTHER PROJECT WITHOUT THE WRITTEN AUTHORIZATION OF BJ DESIGN, LLC

SHEET TITLE:
CONVENIENCE STORE, CARWASH, CANOPY, AND MONUMENT SIGN; EXTERIOR ELEVATIONS

SHEET NUMBER:

A1.0



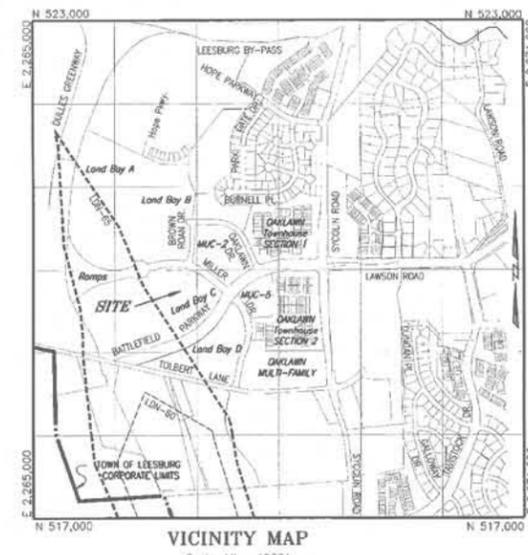
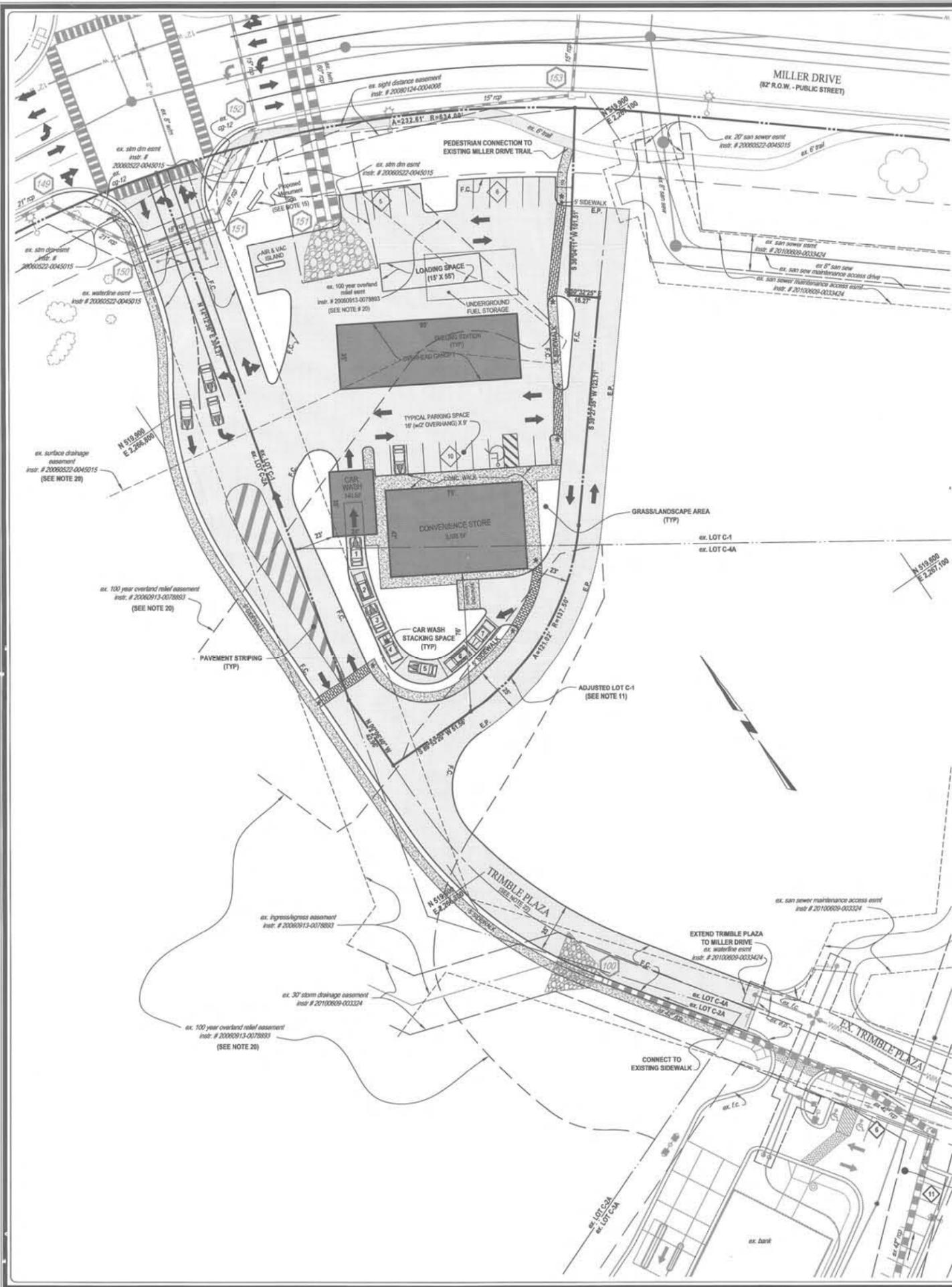
SIGN NOTES:
1) ALL SIGNS ARE FOR DEMONSTRATION PURPOSES ONLY.

SIGN NOTES:
1) ALL SIGNS ARE FOR DEMONSTRATION PURPOSES ONLY.

SIGN NOTES:
1) ALL SIGNS ARE FOR DEMONSTRATION PURPOSES ONLY.

SIGN NOTES:
1) ALL SIGNS ARE FOR DEMONSTRATION PURPOSES ONLY.

SIGN NOTES:
1) ALL SIGNS ARE FOR DEMONSTRATION PURPOSES ONLY.



PARKING TABULATION

REQUIRED: CAR WASH (SELF-SERVE) - (1 SPACE/EMPLOYEE x 6 EMPLOYEES) + 6 SPACES
 CONVENIENCE STORE - (6 SPACES/1000 SQ.FT. x 3,525 SQ.FT.) = 21 SPACES

PROVIDED: 21 SPACES (INCLUDES 1 ACCESSIBLE SPACE)

LOADING SPACE: 1 SEMI-TRAILER LOADING SPACE IS REQUIRED AND PROVIDED

CAR WASH STACKING SPACES: 7 SPACES REQUIRED AND PROVIDED (18' x 10' MINIMUM)

ZONING REQUIREMENTS (PER ZM-159)

REQUIREMENT	REQUIRED	PROVIDED
MINIMUM LOT WIDTH	100'	200'
MINIMUM LOT AREA	20,000 SQ.FT.	50,050 SQ.FT.
MAXIMUM BUILDING HEIGHT	80'	25'
MINIMUM SETBACKS:		
FRONT	35'	100'
SIDE	15'	22'
REAR	15'	75'
MAXIMUM FLOOR AREA RATIO:	0.60 (INDIVIDUAL LOT) 0.40 (PER DISTRICT AS A WHOLE)	0.09

AREA TABULATION

AREA OF ADJUSTED LOT C-1 (AND TOTAL AREA AFFECTED BY THIS SPECIAL EXCEPTION; SEE NOTE 11.)

PROPOSED F.A.R. = 4.365/56.746 = 0.08

CANOPY HEIGHT

MODIFICATION REQUESTED (in accordance with zoning ordinance section 3.4.13) to zoning ordinance section 3.24.F as follows:

Canopy Height as measured from the average finished grade to the lowest point on the canopy fascia should not exceed 17'0". The overall height of canopy shall not exceed 20'6".

LOT C-3A INCLUDES AN EXISTING BANK w/ DRIVE THRU AND RELATED SITE IMPROVEMENTS

- NOTES:**
- ZONING DISTRICTS: PEC AND AI (PER ZM-159 & TLZM-2005-0002).
 - PROPOSED USE: AUTOMOBILE SERVICE STATION w/ CONVENIENCE STORE AND CAR WASH.
 - PROPERTY IDENTIFICATION: THIS SPECIAL EXCEPTION IS LOCATED WITHIN PORTIONS OF THE FOLLOWING:

PIN #	LOT #	Current Area
233-20-0550	(OAKLAWN COMMERCIAL - LOT C-1)	2,567.8 AC.
233-29-6350	(OAKLAWN COMMERCIAL - LOT C-2A)	5,172.9 AC.
233-29-9822	(OAKLAWN COMMERCIAL - LOT C-4A)	2,547.6 AC.
		TOTAL: 10,288.3 AC.
 - ↶ DENOTES TRAFFIC LANES AND/OR DIRECTION OF TRAVEL.
 - THIS SPECIAL EXCEPTION APPLIES TO PORTION OF LANDBAY C DESIGNATED HEREON ONLY.
 - SITE LIGHTING SHALL COMPLY WITH TOWN OF LEESBURG D.C.S.M. AND ZONING ORDINANCE REQUIREMENTS.
 - PERIMETER PARKING LOT LANDSCAPING SHALL COMPLY WITH ZONING ORDINANCE SECTION 12.5. (SEE SHEET 4)
 - INTERIOR PARKING LOT LANDSCAPING SHALL COMPLY WITH ZONING ORDINANCE SECTION 12.6. (SEE SHEET 4)
 - PEDESTRIAN CIRCULATION SYSTEM SHALL COMPLY WITH ZONING ORDINANCE SECTION 11.2.3.B WHERE CROSSING TRAVELWAYS.
 - PEDESTRIAN CIRCULATION SYSTEM SHALL BE LIGHTED IN ACCORDANCE WITH ZONING ORDINANCE SECTION 11.2.4.
 - THE RECONFIGURATION OF PROPOSED LOTS C-1, C-2A AND C-4A SHALL BE APPROVED AND RECORDED PRIOR TO SITE PLAN APPROVAL.
 - DEVELOPMENT IS SUBJECT TO ZONING ORDINANCE USE REGULATIONS 9.3.3 (CAR WASH), 9.3.8 (CONVENIENCE FOOD STORE) AND 9.3.24 (SERVICE STATIONS).
 - PURSUANT TO SECTION 12.8.5.F OF THE ZONING ORDINANCE, THE APPLICANT IS REQUESTING A WAIVER OF THE SCREENING AND BUFFER-YARD REQUIREMENTS ALONG ALL PARCEL BOUNDARIES EXCEPT FOR THE BOUNDARY ALONG MILLER DRIVE. THE MILLER DRIVE BOUNDARY IS SUBJECT TO A PROFFERED LANDSCAPE PLAN UNDER ZMAP #159, WHICH PROVIDES THE REQUIRED SCREENING AND BUFFER-YARD. THE OTHER BOUNDARIES ARE PART OF COMMERCIAL LAND BAY C UNDER THE ZM #159 CONCEPT PLAN IN WHICH ALL USES ARE INTENDED TO INTERRELATE WITH ONE ANOTHER WITHOUT A NEED TO BE SCREENED AND BUFFERED FROM EACH OTHER, AND SINCE THE SCREENING AND BUFFERING IS BEING PROVIDED AROUND THE PERIMETER OF THE LAND BAY ADJACENT TO PUBLIC ROADS UNDER THE PROFFERED LANDSCAPE PLAN FOR ZM #159.
 - SPEAKERS WILL BE LOCATED AT EACH FUELING STATION, AND SHALL COMPLY WITH ZONING ORDINANCE SECTION 7.9.
 - SIGNAGE SHALL COMPLY WITH ZONING ORDINANCE ARTICLE 15. PROPOSED MONUMENT SIGN LOCATION SUBJECT TO SIGN PERMIT APPROVAL.
 - A GREASE-OIL-GRIT-WATER SEPARATOR WILL BE REQUIRED FOR THE PROPOSED CAR WASH. DETAILS SHALL BE PROVIDED WITH THE FINAL SITE PLAN SUBMISSION.
 - STORMWATER RUNOFF QUALITY CONTROL: THIS SITE IS SERVED BY THE STRATFORD LAKE BMP FACILITY. ADDITIONALLY, THIS SITE IS SUBJECT TO THE "HOT SPOT" REQUIREMENTS OF DCSM 5-640 (4) (J) (1) FOR FACILITIES ENGAGED IN STORING OR DISPENSING PETROLEUM PRODUCTS, AND DCSM 5-640 (5), (SWPPP). SEE SHEETS 7 AND 8.
 - APPROVAL OF THE SPECIAL EXCEPTION DOES NOT EXPRESS OR IMPLY ANY WAIVER OR MODIFICATION OF THE REQUIREMENTS SET FORTH IN THE SUBDIVISION AND LAND DEVELOPMENT REGULATIONS, THE ZONING ORDINANCE, OR THE DESIGN AND CONSTRUCTION STANDARDS MANUAL (DCSM) EXCEPT AS SPECIFICALLY APPROVED BY TOWN COUNCIL, OR AS SHOWN AS APPROVED BY TOWN COUNCIL ON THE SPECIAL EXCEPTION PLAN AND SUPPORTING DOCUMENTS.
 - DEVELOPMENT OF THIS SPECIAL EXCEPTION USE SHALL BE IN SUBSTANTIAL CONFORMANCE WITH THIS SPECIAL EXCEPTION PLAN WITH REASONABLE ALLOWANCES TO BE MADE FOR ENGINEERING AND DESIGN ALTERATIONS TO MEET TOWN SUBDIVISION AND LAND DEVELOPMENT REGULATIONS, ZONING ORDINANCE OR DESIGN AND CONSTRUCTION STANDARDS MANUAL REQUIREMENTS.
 - EASEMENT FOR OVERLAND RELIEF AND SURFACE DRAINAGE TO BE MODIFIED WITH THE FINAL SITE PLAN SUBMISSION, IN ACCORDANCE WITH PROPOSED GRADING AND SUPPORTING ENGINEERING CALCULATIONS.
 - * DENOTES LOCATION OF ACCESSIBLE CURB RAMP.
 - TRIMBLE PLAZA IS A VARIABLE WIDTH PRIVATELY MAINTAINED PARKING LOT TRAVEL AISLE.

SHEET INDEX

NO.	DESCRIPTION	DATE
1	SPECIAL EXCEPTION PLAN FOR AUTOMOBILE SERVICE STATION w/ CONVENIENCE STORE AND CAR WASH	
2	LOADING SPACE AUTOTURN ANALYSIS	
3	DUMPSTER AUTOTURN ANALYSIS	
4	LANDSCAPE EXHIBIT	
5	CONCEPTUAL GRADING PLAN	
6	X-SECTIONS	
7	CONCEPTUAL BMP PLAN	
8	CONCEPTUAL STRATFORD LAKE BMP COMPUTATIONS	

DATE: 04-05-2012
 FILE NO: L190.012.B
 DRN: REKSR
 CKD: S.J.W.III

Scale 1" = 30'

NO.	ADDRESS INITIAL TOWN COMMENTS FOR ACCEPTANCE	DATE
2	ADDRESS 1ST TOWN REVIEW COMMENTS	10/12/2012
1	ADDRESS INITIAL TOWN COMMENTS FOR ACCEPTANCE	5/25/2012

SHEET 1 of 8

PACIULLI

SIMMONS & ASSOCIATES
 Established 1744

50 Catocin Circle, NE
 Suite 200
 Leesburg, VA 20176
 PH 703.777.2755
 FX 703.777.8751
 EM leesburg@psalid.com

Engineers
 Planners
 Surveyors
 Landscape Architects
 Wetland Specialists
 Environmental Scientists
 Archaeologists

**OAKLAWN - COMMERCIAL
 LANDBAY C**

TOWN OF LEESBURG
 LOUDOUN COUNTY, VIRGINIA

TLSE-2012-0005

SPECIAL EXCEPTION PLAN
 FOR AUTOMOBILE SERVICE STATION w/
 CONVENIENCE STORE AND CAR WASH

COMMONWEALTH OF VIRGINIA

William

SAMUEL JACKSON WILLIAMS, III
 Lic. No. 017783

10-12-12

PROFESSIONAL ENGINEER

Oaklawn
 11/5
 Comments

STRATFORD LAKE FACILITY DRAINAGE SHED AND LAND USE MAP



LEGEND

- BMP PLANNING AREA
- DRAINAGE DIVIDES PER ULTIMATE DEVELOPMENT TO THE STRATFORD LAKE
- LAND USE SUB-AREA
- UNDEVELOPED PORTION OF SHED NOT COVERED BY STRATFORD LAKE. BMP COVERAGE TO BE PROVIDED ONSITE WITH FINAL DEVELOPMENT

BEST MANAGEMENT PRACTICES (BMP) NARRATIVE & CALCULATIONS

STRATFORD LAKE ADJAINS THE NORTHERN BOUNDARY OF THE OAKLAWN AT STRATFORD DEVELOPMENT. OAKLAWN HAS ACQUIRED PERMISSION FROM THE OWNERS OF THE STRATFORD LAKE FOR UTILIZATION AS A BMP FACILITY. THE FOLLOWING CALCULATIONS, IN ACCORDANCE WITH THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK SECTION 5-10.2, DEMONSTRATE THAT STRATFORD LAKE MEETS THE REQUIREMENTS FOR A RETENTION BASIN II (WITH A TARGET PHOSPHORUS REMOVAL EFFICIENCY OF 50%).

PERFORMANCE-BASED WATER QUALITY CALCULATIONS

STEP 1 - Determine the Applicable Area, (A) and the Post-Developed Impervious Cover, (Ipost).

APPLICABLE AREA, (A):

$$\begin{aligned} \text{Total Site Area} &= 270.1 \text{ AC} \\ \text{Planning Area} &= 263.1 \text{ AC} \\ A &= 263.1 \text{ AC} \end{aligned}$$

POST-DEVELOPED IMPERVIOUS COVER, (Ipost):

Tabulation of Impervious Areas	SF or AC	AC	Ipost = Total Impervious Area / A
Buildings	SF or AC	AC	Ipost = 33.8 AC
Pavement	SF or AC	AC	Ipost = 263.1 AC
Driveways	SF or AC	AC	Ipost = 35.7%
Sidewalks	SF or AC	AC	
Total	4,085,928 SF or	93.8 AC	

STEP 2 - Determine the Existing Impervious Cover, (Iexisting) OR Use the Average Land Cover Condition, (Iwatershed)

EXISTING IMPERVIOUS COVER, (Iexisting):

Tabulation of Impervious Areas	SF or AC	AC	Iexisting = Total Impervious Area / A
Buildings	SF or AC	AC	Iexisting = 0.03%
Pavement	SF or AC	AC	Iexisting = 263.1 AC
Driveways	SF or AC	AC	Iexisting = 0.02
Sidewalks	SF or AC	AC	
Total		N/A	

AVERAGE LAND CONDITION, (Iwatershed):

Iwatershed = 16%

Is the Existing Impervious Cover (Iexisting) served by an existing stormwater management BMP that addresses water quality? NO (Assumption is ex. conditions are prior to lake design & construction)

STEP 3 - Determine the Appropriate Development Situation

DEVELOPMENT SITUATION

SITUATION 2
Land development where the Existing Impervious Cover (Iexisting) is less than or equal to the Average Land Cover (Iwatershed) and the proposed improvements will create a total Percent Impervious Cover (Ipost) which is greater than the Average Land Cover Condition (Iwatershed).

REQUIREMENT

The Post-Development Pollutant Discharge (Ipost) shall not exceed the Existing Pollutant Discharge based on the Average Land Cover Condition (Iwatershed).

STEP 4 - Determine the Relative Pre-Development Pollutant Load, (Ipre)

$$\begin{aligned} Ipre\text{-existing} &= 10.05 + (0.009 \times Iexisting) \times A \times 2.28 \\ Ipre\text{-existing} &= 10.05 + (0.009 \times 0.0) \times 263.1 \times 2.28 \\ Ipre\text{-existing} &= 30.8 \text{ LB/YR} \\ Ipre\text{-watershed} &= 10.05 + (0.009 \times Iwatershed) \times A \times 2.28 \\ Ipre\text{-watershed} &= 10.05 + (0.009 \times 16.0) \times 263.1 \times 2.28 \\ Ipre\text{-watershed} &= 116.4 \text{ LB/YR} \end{aligned}$$

STEP 5 - Determine the Relative Post-Development Pollutant Load, (Ipost)

$$\begin{aligned} Ipost &= 10.05 + (0.009 \times Ipost) \times A \times 2.28 \\ Ipost &= 10.05 + (0.009 \times 35.7) \times 263.1 \times 2.28 \\ Ipost &= 222.5 \text{ LB/YR} \end{aligned}$$

STEP 6 - Determine the Relative Pollutant Removal Requirement, (RR)

$$\begin{aligned} RR\text{-existing} &= Ipost - (0.9 \times Ipre\text{-existing}) \\ RR\text{-existing} &= 222.5 - (0.9 \times 30.8) \\ RR\text{-existing} &= 195.3 \text{ LB/YR} \\ RR\text{-watershed} &= Ipost - Ipre\text{-watershed} \\ RR\text{-watershed} &= 222.5 - 116.4 \\ RR\text{-watershed} &= 106.1 \text{ LB/YR} \end{aligned}$$

$$\begin{aligned} RR &= Ipost - Ipre\text{-watershed} \\ RR &= 106.1 \text{ LB/YR} \end{aligned}$$

STEP 7 - Identify the Best Management Practice (BMP) Options for the Site

REQUIRED REMOVAL EFFICIENCY:

$$\begin{aligned} EFF &= (RR / Ipost) \times 100 \\ EFF &= \frac{106.1}{222.5} \times 100 \\ EFF &= 48\% \end{aligned}$$

STRUCTURE	TYPE (SIZE)	Approx. Area (AC)	Impervious Area (AC)	Flow (CFS)	Flow (MGD)	Flow (MGD)	Removed (LB/YR)
Retention Basin	263.1	93.8	93.8	222.5	50	111.2	111.2

$$\begin{aligned} \text{Removed/total} &= \frac{111.2 \text{ LB/YR}}{106.1 \text{ LB/YR}} \\ RR &= 106.1 \text{ LB/YR} \end{aligned}$$

Removed-total > RR — THEREFORE, BMP REQUIREMENTS MET

IMPERVIOUS AREA TO LAKE TABULATION

#	LAND USE SUB-AREA	TOTAL AREA	AVERAGE (I) IMPERVIOUS AREA	IMPERVIOUS AREA
1	OAKLAWN PARK	4.1 AC.	(2)	0.2 AC.
2	OAKLAWN TOWNHOUSES SECTION 1	3.4 AC.	(2)	1.2 AC.
3	OAKLAWN TOWNHOUSES SECTION 2	1.8 AC.	(2)	1.0 AC.
4	OAKLAWN TOWNHOUSES SECTION 3	3.1 AC.	68% (1)	2.1 AC.
5	PRC MIXED USE (MUC5)	4.2 AC.	(2)	2.2 AC.
6	OAKLAWN (LANDBAY D)	15.3 AC.	(2)	8.6 AC.
6A	HOPE PARKWAY	5.3 AC.	(2)	4.3 AC.
7	OAKLAWN COMMERCIAL (LANDBAY C)	10.7 AC.	80% (1)	8.6 AC.
7A	OAKLAWN COMMERCIAL (LANDBAY C LOT C-3A)	2.4 AC.	(2)	1.4 AC.
7B	OAKLAWN COMMERCIAL (LANDBAY C GAS STATION)	1.5 AC.	(2)	1.2 AC.
8	OAKLAWN COMMERCIAL (LANDBAY A)	14.9 AC.	65% (1)	9.7 AC.
9	OAKLAWN COMMERCIAL (LANDBAY B)	16.9 AC.	65% (1)	11.0 AC.
10	PRC MIXED USE (MUC 2)	11.4 AC.	65% (1)	7.4 AC.
11	OPEN SPACE	23.6 AC.	0% (1)	0 AC.
12	ADDITIONAL ROAD R.O.W.	3.5 AC.	(2)	1.9 AC.
13	DULLES GREENWAY	28.1 AC.	(2)	9.5 AC.
14	FREEDOM PARK	9.8 AC.	(3)	1.8 AC.
15	DETACHED RESIDENTIAL	16.4 AC.	(3)	4.3 AC.
16	SIMPSON MIDDLE SCHOOL	28.0 AC.	(3)	7.0 AC.
17	AIRPORT	23.2 AC.	(3)	0.3 AC.
18	EAST STRATFORD (LANDBAY E)	2.7 AC.	(2)	2.0 AC.
19	EAST STRATFORD (LANDBAY D)	13.5 AC.	(3)	5.0 AC.
20	EAST STRATFORD (PHASE B)	8.6 AC.	(3)	2.8 AC.
21	STRATFORD LAKE	10.7 AC.	(2)	0.4 AC.
TOTAL		263.1 AC.	35.7%	93.8 AC.

- (1) AVERAGE IMPERVIOUS COVER ESTIMATED BASED ON OAKLAWN CONCEPT PLANS.
- (2) IMPERVIOUS AREA SHOWN PER CONSTRUCTION PLANS FOR THE SITE INDICATED IN THE LAND USE COLUMN.
- (3) IMPERVIOUS AREA SHOWN PER GIS DATA AND VISUAL OBSERVATIONS FOR SITE INDICATED IN THE LAND USE COLUMN.

STRATFORD LAKE DESIGN DATA

(PER STRATFORD-RYLAND LAKE FACILITY PLANS PREPARED BY DEWBERRY & DAVIS.)

- AT NORMAL POOL: ELEVATION = 330.00
- AS-BUILT AREA = 6.07 ACRES (PER DEWBERRY 10-20-03)
- DEPTH = APPROXIMATELY 15% OF LAKE IS 0'-1.5' DEEP. MOST OF LAKE IS 2'-7' DEEP.
- AS-BUILT VOLUME = 31.06 AC.FT. (PER DEWBERRY 10-20-03)

WATER QUALITY VOLUME CALCULATION

(PER SECTIONS 3.06 & 5-6 OF THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK)

$$\begin{aligned} \text{WQV} &= \text{IMP AREA (ft}^2) \times 1/2 \text{ in} / 12 \text{ in/ft} \\ &= 4,085,928 \text{ ft}^2 \times (1/2 \text{ in}) / (12 \text{ in/ft}) \\ &= 170,247 \text{ ft}^3 \\ &= 3.91 \text{ ac.ft.} \\ 4 \times \text{WQV} &= 15.63 \text{ ac.ft. (MINIMUM REQUIRED)} \\ 15.63 \text{ ac.ft.} &< 31.06 \text{ ac. ft. (PROVIDED PER DEWBERRY 10-20-03)} \end{aligned}$$

CONCLUSION:

NO MODIFICATIONS TO THE STRATFORD LAKE ARE NECESSARY PER SECTION 3.06 OF THE VIRGINIA STORMWATER MANAGEMENT HANDBOOK. LAKE IS SIZED ADEQUATELY TO ACT AS A RETENTION BASIN II WITH A TARGET PHOSPHORUS REMOVAL EFFICIENCY OF 50%.

PACIULLI

SIMMONS & ASSOCIATES

50 Catocin Circle, NE
Leesburg, VA 20176
PH 703.777.2755
FX 703.777.8751
EM leesburg@psaltd.com

Engineers
Planners
Surveyors
Landscape Architects
Wetland Specialists
Environmental Scientists
Archaeologists

OAKLAWN - COMMERCIAL
LANDBAY C
TOWN OF LEESBURG
LOUDOUN COUNTY, VIRGINIA

CONCEPTUAL
STRATFORD LAKE BMP
COMPUTATIONS

COMMONWEALTH OF VIRGINIA
SAMUEL JACKSON WILLIAMS, III
Lic. No. 017783
10-12-12
PROFESSIONAL ENGINEER

DATE: 04-05-2012
FILE NO: L190.012.B
DRN: REKSR
CKD: S.J.W.III

SHEET 8

Oaklawn, Landbay C
Special Exception Application for an
Automobile Service Station, Convenience Store and Car Wash
Written Statement
April 3, 2012
Revised October 12, 2012

I. Introduction

Oaklawn Development Partners, LLC is filing a special exception application for an automobile service station, convenience store and car wash on 1.15 acre tract of land within Land Bay C of the Oaklawn Planned Employment Community (PEC) development. The 1.15 acres currently is part of 3 separate parcels that will be consolidated into one parcel subsequent to the approval of the proposed special exception application. The Oaklawn PEC district is subject to the proffers for ZM #159 and TLZM 2005-0002. The Oaklawn PEC has been planned for two service stations within the community; however, the proffers for TLZM 2005-0002 stipulated that a special exception would be required if one the of the service stations were to locate in Land Bay C.

II. Type of Operation

A service station is proposed with eight fueling stations containing 16 gas pumps. Along with the service station will be a 3,525 square foot convenience food store and an 825 square foot car wash.

III. Hours of Operation

This facility will operate 24 hours a day, seven days a week.

IV. Traffic Impacts

The traffic studies submitted for ZM #159 and TLZM 2005-0002 analyzed the proposed special exception use. The most recent traffic study analyzed a gas station in Land Bay C as one of the two gas stations within the Oaklawn development. Oaklawn has proffered and phased road improvements based on this study. The impacts of the proposed special exception uses were accounted for in these prior studies and in the proffered and phased road improvements.

V. Impacts on adjacent uses and measures proposed to mitigate such impacts.

The governing Oaklawn PEC zoning with concept plan and proffers created a comprehensively planned community that accounted for a variety of uses needed to create a thriving employment center. Two gas stations always have been contemplated within Oaklawn. In Land Bay C, the proposed special exception site is surrounded by future uses that could include pharmacy, restaurant, and office, office, light industrial, data center, free-standing restaurant, fast food with drive through and bank with drive-through. One of the Mixed-Use Center land bays is located across Miller Drive from the proposed special exception uses, which is planned for similar uses proposed in this application, as well as office and residential uses. Oaklawn has been planned with a unified landscaping plan along the public streets, and the special

exception uses will comply with the unified landscaping plan along Miller Drive. The uses internal to Land Bay C are intended to be integrated with one another using a coordinated private driveway network.

VI. Compliance with Section 3.4.12 Approval Criteria.

A. The proposed use will not adversely affect the use of neighboring properties.

An amendment to TLZM 2005-0002 in 2007 relocated the 2nd gas station from MUC 2 to Land Bay C for the purpose of moving it further from the residential units in Oaklawn. This proposed Special Exception fulfills the commitment of that proffer amendment. As for adjacent uses, the gas use is fully compatible with the other planned uses such as office, retail, bank, restaurant, pharmacy and personal service.

B. The use shall comply with applicable zoning district regulations and applicable provisions of the adopted *Town Plan*, including but not limited to the Plan's Land Use Compatibility policies.

Land Bay C within Oaklawn is designated for regional office use on the Planned Land Use Map of the Town Plan. Oaklawn is a planned employment community for which the concept plan accommodated locations for the primary uses, as well as the supporting uses. The concept plan for Land Bay C, in particular, designated restaurant, bank and service station uses along with the office uses as part of this regional office community. The regional office designation contemplates the need for retail and service uses for the daily needs of the workers, customers and businesses in the development. This special exception use proposes to provide such uses as contemplated in the concept plan for Oaklawn. Development within Oaklawn is subject to the H-2 Corridor Design Guidelines to ensure architectural compatibility among the various uses within Oaklawn.

C. The location, size and height of buildings, structures, walls and fences, and the nature and extent of screening, buffering and landscaping shall be such that the use will not hinder or discourage the appropriate development and use of adjacent or nearby land and/or buildings.

The proposed uses have been planned as part of the other Land Bay C uses, which include an office building, restaurant and pharmacy uses surrounding the proposed uses. The internal private street network accommodates these uses, and none of the uses interfere with each other's operations. The proposed buildings will be one-story, and the service station canopy will comply with the Zoning Ordinance maximum height requirement. Oaklawn has a unified landscape plan bordering all of the public streets within Oaklawn. The site containing the proposed uses fronts on Miller Drive, and the Miller Drive landscaping will be provided in accordance with the required buffer and the Oaklawn landscape plan. A six-foot wide trail also will be constructed within the setback along Miller Drive as part of the Oaklawn trail system.

- D. The proposed use will be such that pedestrian and vehicular traffic generated will not be hazardous or conflict with the existing and anticipated traffic in the neighborhood and on the streets serving the site.**

Both the vehicular and pedestrian access is part of the networks planned for the Oaklawn planned development. The proposed special exception uses have been contemplated as part of the Oaklawn planned development from its inception; therefore, the traffic generated by these uses was anticipated in the initial planning for Oaklawn.

VII. Compliance with Section 9.3, Use Standards

A. Section 9.3.3 Car Washes

- A. Car washes shall be located and designed so that vehicular circulation shall not conflict with traffic movements in adjacent streets, service drives, driveways and or parking areas.*

The car wash entrance is accessed from an internal driveway that serves this use. The vehicles exiting the car wash will use the site entrance that accesses the internal private road serving Land Bay C. There is no direct access to the public street network.

- B. Car washes (as an accessory use) shall have no separate and exclusive curb cut access to the abutting roadway.*

The proposed accessory car wash use has an access point from a driveway and vehicles exiting the car wash will use the site's entrance onto the internal private road serving Land Bay C. There is no direct access to the public street network.

B. Section 9.3.6 Convenience Food Store

- A. No convenience food store shall be located on a lot adjacent to residential dwelling units.*

There are no dwelling units located on the lots adjacent to the proposed special site.

- C. All business services, storage and display of goods shall be conducted within a completely enclosed building.*

The applicant shall adhere to this requirement once the convenience food store is operating.

D. Section 9.3.21 Service Stations

- A.** *Service stations shall not include ancillary uses such as vehicular or tool rental (including moving vans) and shall be limited to the servicing of vehicles and nonautomotive accessory retail sales of snacks, convenience foods, and similar products.*

Vehicle servicing is not proposed with this application nor is vehicle rentals. Retail sales will occur as part of the convenience food store use.

- B.** *Service stations shall not include an outdoor storage area for more than three (3) abandoned, wrecked or inoperable vehicles on the site for more than one week, subject to the limitation that there shall be no dismantling, wrecking, or sale of said vehicles or part(s) thereof.*

There will be no inoperable vehicles, since servicing of vehicles is not proposed with this service station.

- C.** *Outside sales and display areas shall be shown on the plan to be reviewed by the Planning Commission and Town Council.*

Outside sales and display are not proposed with this use.

- D.** *The parking of commercial vehicles shall be prohibited on the site of a service station. This provision shall not be interpreted as prohibiting the parking of vehicles actively engaged in delivering fuel or other supplies to the service station.*

The applicant will comply with this requirement.

- E.** *No permit shall be issued for a service station if the entrance or exit for vehicles is*

- 1.** *In the same block front within 200 feet of any school, public playground, hospital, church, or public library; or*

None of these uses are included in Oaklawn.

- 2.** *If such entrance or exit is be located within 20 feet of an "R" District within the same block front.*

The entrance for the service station is internal to Land Bay C, which does not contain any R District zoning.

- 3.** *No permit shall be issued for a service station if any part of any structure, including underground gasoline tanks or service aisles, is located within 100 feet of any*

Oaklawn Commercial – Land Bay C
Automobile Service Station with Car Wash and Convenience Retail Special Exception
Written Statement

April 3, 2012 Revised October 12, 2012

Page 5 of 5

building or grounds of a school, public playground, hospital, church, or public library or any "R" District.

None of these uses exist within Oaklawn.

- F.** *Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".*

The proposed canopy will comply with these height requirements. A modification request pursuant to Section 3.4.3 of the Zoning Ordinance has been submitted to modify the canopy height fascia not to exceed 17'0" and the overall height not to exceed 20'6".

- G.** *Canopy lighting shall be fully recessed in the ceiling of the canopy.*

The proposed canopy lighting will comply with this requirement.

- H.** *All sides of a building should express consistent architectural detail and character. All site walls, screen walls, garage doors and canopies should be architecturally integrated with the building by using similar materials, color and detailing.*

Oaklawn is subject to the H-2 Corridor Design Guidelines and process. Those guidelines and process can ensure that this provision will be adhered to by the proposed uses.

TLSE 2012-000⁸⁵
Oaklawn Commercial Landbay C
Automobile Service Station with Car Wash and Convenience Retail Special Exception

ZONING ORDINANCE MODIFICATION REQUESTS

October 12, 2012

The applicant respectfully requests the following zoning ordinance modifications:

1. Pursuant to Section 3.4.13, Compliance with Use Standards, a modification by the Town Council of Section 9.3.24.F, Automobile Service Station Canopy Height.

Zoning Ordinance Section to be Modified:

Section 9.3.24 Use Standards – Service Stations

F. Canopy Height as measured from the finished grade to the lowest point on the canopy fascia should not exceed 14'0". The overall height of canopies shall not exceed 17'3".

Proposed Modification:

Canopy Height as measured from the average finished grade to the lowest point on the canopy fascia should not exceed 17'0". The overall height of the canopy shall not exceed 20'6".

Applicant's Justification:

The applicant has consulted with a gas station operator who recently constructed a service station in Ashburn and who likely will be the operator of this gas station. His architect has told the applicant that his delivery trucks cannot accommodate the height limits specified in this section of the Zoning Ordinance. The proposed grading for the property also contributes to this modification request. The site slopes downward to the east, which means the distance between the canopy and finished grade on its eastern side is greater than that distance on the western side, since the canopy must be level across its top. Having an appropriate canopy height with minimum clearances is necessary as a safety factor in the operation of the service station.

2. Pursuant to Section 12.8.5.F, a modification by the Town Council of Section 12.8.3, Buffer Yard Matrix, concerning screening and buffer yard requirements.

Zoning Ordinance Section to be Modified:

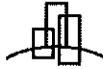
*Section 12.8.3 Buffers and Screening. Screening and Buffer-Yard Matrix.
Cc adjacent to Cb: 10 foot buffer yard with S2 screening.*

Proposed Modification

Eliminate the ten-foot buffer yard and S2 plantings along Trimble Plaza and the eastern parcel boundary, and provide the landscaping buffer proffered under ZM# 159 along Miller Drive.

Applicant's Justification:

ZM #159 was approved with a landscape scheme whereby the landbays within Oaklawn, which are defined by the surrounding public street network, would display a cohesive landscape scheme around the periphery of each landbay and along each public street. Alternatively the landscaping within each landbay would be provided to enhance the appearance of the mix of uses within each landbay, and not necessarily to separate and demarcate each use by a landscaped strip. Users locating in each landbay are a component of a larger entity with shared access drives and not separate physical entities unto themselves needing separation. The Oaklawn automobile service station proposal falls under this concept. It is providing the 35-foot landscaped strip along Miller Drive that has been provided according to the overall Oaklawn Commercial landscape plan. Landscaped areas are provided along Trimble Plaza meeting the S2 screen width; however, those areas are being used to provide the required interior parking lot landscaping. The eastern side of the parcel is the side that is to be integrated with the future adjacent uses in Landbay C, which the applicant submits does not need to be separated by a landscape buffer.



WALSH COLUCCI
LUBELEY EMRICH
& WALSH PC

Christine Gleckner, AICP
Land Use Planner
(571) 209-5776
cgleckner@ldn.thelandlawyers.com

October 12, 2012

Via Hand Delivery

Irish Grandfield, Planner
Leesburg Department of Planning and Zoning
25 West Market Street, SW
Leesburg, VA 20176

RE: Special Exception TLSE-2012-0005, Oak Lawn Gas Station

Dear Mr. Grandfield:

This letter addresses and provides you with a written response to the referral agency comments in the above referenced application. For your convenience, each of the staff comments are stated below and the Applicant's responses follow in bold italics.

I. General

1. **Statement of Justification:** Part VI.C. states "The proposed uses have been planned as part of the other Land Bay C uses, which includes an office building, restaurant and pharmacy uses surrounding the proposed uses." This is verbatim from a previously submitted SE application that was withdrawn which included these specific uses as part of a proposed site plan. That site plan (TLPF-2008-0001) was later modified and dropped those uses before approval. Therefore, per the approved Rezoning for Land Bay C (TLZM 1999-0159), uses may include: office, light industrial or data center, free standing restaurant, fast food w/drive-through, car wash or bank w/drive-through. The applicant should reference the approved Rezoning uses and not the uses currently listed in their Written Statement unless applicant can demonstrate that the listed uses will be the adjacent uses. TLZO 3.4.6.E.10. *(DPR Comment 32, Attachment 2)*

Applicant Response: *The written statement has been revised to reference the uses approved with the rezoning.*

2. **Lot Configuration Suggestion:** The proposed lot boundaries are not consistent with Oaklawn Land Bay D because the lot lines do not coincide with the centerline or

PHONE 703 737 3633 ■ FAX 703 737 3632 ■ WWW.THELANDLAWYERS.COM
1 E. MARKET STREET, THIRD FLOOR ■ LEESBURG, VA 20176-3014

ARLINGTON OFFICE 703 528 4700 ■ PRINCE WILLIAM OFFICE 703 680 4664

ATTORNEYS AT LAW

boundary of the proposed private streets and shared travelways. Applicant should consider revising the west, south and eastern property lines of the proposed lot to follow along the centerlines of the travelways of the bordering private streets. This might provide practical advantages to subsequent purchasers of the lot. If this revision is made, Applicant should also revise the area calculations, setbacks and labels on plan per “new” area and provide ties from the existing boundary lines shown to the proposed boundary. (*DPR Comment 2, Attachment 2*)

Applicant Response: *The lot boundaries have been revised as recommended by staff.*

- H-2 Historic District Guidelines:** First review has been completed. Upon 2nd submission the elevations will be conveyed to the Board of Architectural Review (BAR) in accordance with the current Town review process. BAR approval will be required for this project in accordance with the proffers of TLZM-1999-0159. (*Zoning Comment 1, Attachment 1*)

Applicant Response: *Proposed elevations are included with this submission for the BAR referral. Please let us know at which BAR meeting this referral will be considered so that we may attend.*

II. Town Plan Conformance

- Site Layout:** The layout and design of the proposed use does not meet the standards intended for regional office areas. Specifically, the Town Plan states “Site design should conform to H-2 district design guidelines” (Site Design and Location Criteria #6, p. 50). The H-2 Corridor Design Guidelines (pp. 32-33) call for “consolidating vehicular entrances/exits to a site” to limit traffic conflicts. Further, “entrances and driveways should permit safe and convenient pedestrian crossing where they intersect sidewalks and other pedestrian circulation systems.” In this case, the proposal has three two-way access points on a small lot creating conflicts with pedestrians who will be walking to this site and to other future uses in this Land Bay. Fewer entrances and more uninterrupted sidewalks would better achieve the design goals of Regional Office. Staff recommends that the site design be reconsidered to more adequately address vehicular and pedestrian functionality. (*Project Manager Comment, no attachment*)

Applicant Response: *By virtue of the proposed use as an automobile service station, this use requires vehicular functionality while also accommodating pedestrians. The site entrances have been designed to permit adequate vehicular flow on the property avoiding the stacking that would occur with fewer vehicular access points which could block access to the fueling stations. A through flow pattern also is required for the fuel trucks serving the property without needing to back-up or turn-around on site. Pedestrians are being accommodated with clearly delineated sidewalks and crosswalks to access the site safely.*

- Gateway Design:** Town Plan guidance is that Regional Office development should be compatible with Leesburg’s character as established by the more traditional urban core (Regional Office Intent Statement #5, page 50). This service station will lie off of the exit ramp from the Dulles Greenway not far from the intersection with Battlefield Parkway, a

major entry point into the Town from the south. The proposed building set back of 200 feet from the road fronted by a large overhead canopy is a conventional gas station design that is typically suburban in orientation rather than traditional town design like what is expected for a Gateway Corridor in a development planned as Regional Office. Staff recommends that to better meet the goals of Regional Office, the site layout should be reconfigured so that the convenience store is located adjacent to Miller Drive to screen the parking and fuel pumps from the road; the fueling station should be relocated on the interior of the site with the car wash remaining in its present location. (*Project Manager comment, no attachment*)

Applicant Response: *The applicant has determined that the proposed site lay-out best meets the needs of the proposed use. The proposed lay-out is consistent with the lay-out for all of the existing gas stations in Leesburg for the reason that the proposed lay-out works best for this use. The landscaping along Miller Drive and undergoing review by the BAR consistent with the H-2 guidelines are the elements of this proposal to assure appropriate gateway design.*

III. Plat Issues

6. **Canopy Height.** TLZO Sec. 9.3.24.F states that the overall height of the canopy cannot exceed 17'3" above the finished grade. An examination of the profile on Sheet 6 indicates the canopy height as approximately 19'. This must be corrected. (*Project Manager Comment, no attachment*)

Applicant Response: *A modification request for the canopy height pursuant to Zoning Ordinance Section 3.4.13 has been included with this submission. The modification is noted on Sheet 1.*

7. Notes and Tabulations:

- a. ***Title:*** On Sheet 1 per SLDR Sec. 3.18b.(1)((a)), add the LMIS identification number "TLSE-2012-0005" to the coversheet. See attached example. (*DPR Comment 1, Attachment 2*)

Applicant Response: *The application number has been added to Sheet 1.*

- b. ***Cover Sheet:*** For the "Minimum setbacks provided", change front 99 feet to 106 feet. (*Zoning Comment 5, Attachment 1*)

Applicant Response: *The minimum setbacks provided have been revised as recommended by staff.*

- c. ***Incorrect References:*** To indicate the appropriate sections, please make the following revisions: Note 12 change Sec 9.3.6 reference to 9.3.8; Change Sec. 9.3.21 reference to 9.3.24; and Note 13 change Sec. 12.8.F reference to 12.8.5 F. (*Zoning Comment 4, Attachment 1*)

Applicant Response: *The note references have been revised as recommended by staff.*

- d. Arrows: To avoid confusion, revise the plat and Note 4 on Sheet 1 as necessary because the arrows shown on the plat represent more than just “proposed means of ingress and egress to property from a public street”. DCSM Sec.10-120.2J(6). (DPR Comment 7, Attachment 2)

Applicant Response: Note 4 on Sheet 1 has been revised as recommended by staff.

- e. Note 11: Revise the note on Sheet 1 to read “The reconfiguration of proposed Lots C-1, C-2A and C-4A shall be approved and recorded prior to site plan approval”. SLDR Sec. 2.04. (DPR Comment 8, Attachment 2)

Applicant Response: Note 11 on Sheet 1 has been revised as recommended by staff.

- f. Note 16: Revise the note on Sheet 1 to read “A grease-oil-grit-water separator...” Also show the general location of the proposed structure on the plan. (Sht. 1) TLZO Sec. 3.4.6.E.3. (DPR Comment 9, Attachment 2; Utilities Comment 1, Attachment 5)

Applicant Response: Note 16 on Sheet 1 has been revised as recommended by staff. The general location of the separator is shown on Sheet 5.

- g. Update: The statement currently shown on Sheet 1 of the plat: “storm junction box by others” is inaccurate. The proposed junction box will need to be constructed by the owner(s) of this property in order to provide a complete storm sewer connection/outfall. DCSM Sec. 10-120.2J(6). (DPR Comment 17, Attachment 2)

Applicant Response: The statement has been removed. The applicant acknowledges that the junction box will need to be constructed with this project.

- h. Labels: To ensure that utilities are laid out in an adequate manner for this site, staff recommends that all lines on sheets be labeled including providing sizes for existing pipes. For example, provide a label for the 100-yr. overland relief path on Sheet 1. Also label all existing easements and provide recordation references on Sheets 1-5 per TLZO Sec. 3.4.6.E.20. (DPR Comment 10 & 14, Attachment 2)

Applicant Response: Sheet 1 has been revised to reflect existing pipe sizes and easements as recommended by staff.

8. Other Plat Revisions:

- a. Dimensions: Provide complete exterior dimensions of all proposed buildings and structures on the plan on Sheets 1-5 per TLZO Sec. 3.4.6.E.18. (DPR Comment 5, Attachment 2)

Applicant Response: Dimensions have been added to Sheet 1 as recommended by staff.

- b. Landscape Exhibit: Revise the Landscape Exhibit to include the concept grading, proposed/existing utilities and the associated easements with the proposed

landscaping overlaid to avoid potential conflicts at the time of site plan review. (Sheets 1-5) TLZO Sec. 3.4.6.E.20. (DPR Comment 11, Attachment 2)

Applicant Response: *Sheet 4 has been revised as recommended by staff.*

- c. Site Grading: Label CG-6 and EP designations on the plan on Sheets 1-5 per TLZO Sec. 3.4.6.E.23. (DPR Comment 24, Attachment 2)

Applicant Response: *The face of curb lines (F.C.) have been labeled on Sheet 1, as has the edge of pavement line (E.P.) along the eastern edge of the access driveway. The curb type will be specified on the site plan.*

- d. SWM Structure: Clarify the type of structure labeled "Ex. 151" as either as a curb inlet or junction box or a joint structure (Junction Box with Curb Inlet top) on Sheet 5 per TLZO Sec. 3.4.6.E.23. (DPR Comment 16, Attachment 2)

Applicant Response: *Sheet 5 clarifies that the structure is a junction box.*

- e. Roadway Design: Provide the typical sections for both Trimble Way as well as the access way to the east on the plat on Sheets 1-6 per TLZO Sec. 3.4.6.E.25. (DPR Comment 25, Attachment 2)

Applicant Response: *Trimble Way, a private access travel aisle and not a public street, varies in width, which means a "typical" section cannot be provided. The dimensions of the travel aisles and lane configurations, however, are shown on Sheet 1 of the plat.*

- f. Lawn vs. Hardscape: Graphically differentiate between grass/landscaped areas and concrete/pedestrian paths on the plat. (Sheets 1-5) per DCSM Sec. 10-120.2E(1). (DPR Comment 4, Attachment 2)

Applicant Response: *Sheets 1 and 4 have been revised to provide shading on the hardscaped areas of the plat.*

IV. Landscaping and Buffer Issues

9. **Landscaping/Screening:** Sheet 1, Note 13, fourth sentence states: "The other boundaries are part of commercial Land Bay C under the ZM#159 concept plan in which all uses are intended to interrelate with one another without a need to be screened and buffered from each other [emphasis added], and since the screening and buffering is being provided around the perimeter of the land bay adjacent to public roads under the proffered landscape plan for ZM#159". The Rezoning Concept Plan/proffers do not include this language and the Zoning Administrator has not interpreted that the existing Concept Plan waived all other landscaping requirements in the Oaklawn planned development. Therefore, the applicant will need to show buffering and screening consistent with Article 12 on the Zoning Ordinance or the applicant will need to submit a modification request to reduce or eliminate the required landscaping under TLZO Sec. 12.8.5.F. In any case, this note must be revised as necessary or deleted. (Project Manager Comment, no attachment; DPR Comment 28, Attachment 2)

Applicant Response: *As stated in Note 13, the applicant is requesting a modification of the buffer yard requirements with this special exception application pursuant to Section 12.8.F of the Zoning Ordinance. The modification request is included with this submission.*

10. Potential Landscaping/Utilities Conflicts: To ensure that proposed landscaping can actually be planted where indicated on Sheet 4 Landscape Exhibit, it is necessary to identify potential impacts to landscaping. Therefore, show the following on Sheet 5 Conceptual Grading Plan:

- a. Location of proposed water service to the store and the car wash;
- b. Location of the sand/grease interceptor; and
- c. Any proposed underground storage tanks for recycled water associated with the car wash. (*Utilities Comments 1, 2, 6, Attachment 5*)

Applicant Response: *Sheet 5 has been revised to include the information items requested by staff.*

V. Transportation Issues

11. Left Turn Lane onto Miller Drive: Revise the pavement markings on Trimble Plaza to eliminate the southbound left turn lane entering the site. All available storage area is needed for the northbound left turn on to Miller Drive. One possible solution would be to add a second southbound through lane that tapers back to a single lane past the first gas station entrance. (*DPW Transportation Comment 1, Attachment 3*)

Applicant Response: *The plat has been revised to provide the left turn lane from Trimble Plaza onto Miller Drive with stacking back to the service station entrance. Additionally, a short left turn on Trimble Plaza has been provided for vehicles entering the service station entrance.*

12. Traffic Circulation: The right/left turn lanes at the western side of Trimble Way do not appear to function properly in the interim or ultimate conditions. Therefore, a complete updated traffic study will be required prior to any future development in Land Bay C beyond this gas station proposal. TLZO Sec. 3.4.6.G. (*Project Manager Comment, no attachment; DPR Comment 22, Attachment 2*)

Applicant Response: *Applicant will comply with site plan requirements for future developments.*

13. Roadway Classification: Provide a designation on the plan to clarify Trimble Plaza as a "Privately Maintained Parking Lot Travel Aisle" or provide another suitable label on Sheets 1-5 per TLZO Sec. 3.4.6.E.7. (*DPR Comment 23, Attachment 2*)

Applicant Response: *Trimble Plaza has been labeled as recommended on Sheet 1 of the plat.*

14. Roadway Network Configuration: The proposed intersection midway on Trimble Plaza shows a curved departure to the west. There is nothing currently shown on other parts of the landbay which would require such a curved intersection at this point. Therefore,

either reconfigure the intersection to be 90° to the centerline of roadway or provide additional information to explain the need for the current configuration. Provide 100' of tangent (mirroring the east side of the intersection) on the west leg extension (Sheets 1-5) per TLZO Sections 3.4.6.E.7 and 3.4.14.C. (*DPR Comment 20, Attachment 2*)

Applicant Response: *The leg of the intersection in question has been removed from the plat. As discussed at the meeting with staff, there are no geometric requirements for this intersection. The configuration of that travelway will be determined when that land bay is developed.*

- 15. Turn Lanes:** Because the future traffic volume exiting this landbay has not been determined, staff recommends that the proposed street/intersection layout be revised to allow for future right turn/left turn lanes which may be required to serve the future development of this landbay (Sheets 1-5) per TLZO Sections 3.4.6.E.7 and 3.4.14.C. (*DPR Comment 22, Attachment 2*)

Applicant Response: *Comment noted.*

VI. Parking/Vehicular/Pedestrian Circulation Issues

- 16. Parking Spaces Location:** The four (4) parking spaces on the eastern portion of the lot that back into a travel aisle appear to present an unsafe condition. The close proximity of these spaces to the loading space and dumpster location, and the entry way and stacking spaces for the carwash, could present difficulty to the users, especially when coupled with the fact that these four spaces also back out onto a multi-user roadway. This is not the same as backing onto an internal to the site (single user) travel aisle. Also, these four spaces are not required for the uses proposed on site. Therefore, the four parking spaces should be eliminated or redesigned. (*Zoning Comment 3, Attachment 1; DPR Comment 30, Attachment 2*)

Applicant Response: *Sheet 1 has been revised to remove these parking spaces as recommended by staff and replaced with a landscape area.*

- 17. Pedestrian Access:** Show the location of all proposed CG-12's at street and pavement crossings at all public road intersections as well as at all internal pedestrian crossings at private intersections (Sheets 1-5) per TLZO Sec. 3.4.6.E.7. (*DPR Comment 18, Attachment 2*)

Applicant Response: *Note 21 has been added to Sheet 1 denoting the symbol used to delineate accessible curb ramps.*

- 18. Accessible Parking Spaces:** Delineate (stripe) the handicap parking access aisle on the east side of the van accessible handicap parking space. Also update the parking tabulations to show the revised number of spaces provided on Sheet 1 per TLZO Sec.11.6.2.B.2. (*DPR Comment 19, Attachment 2*)

Applicant Response: *Sheet 1 has been revised as recommended by staff.*

19. **Stacking:** Please note that the carwash only has 6 stacking spaces instead of the 7 required by TLZO Sec. 11.8.2. The carwash bay cannot count as a stacking space. (*Zoning Comment 2 Attachment 1; DPR Comment 28, Attachment 2*)

Applicant Response: *Sheet 1 has been revised to provide seven stacking spaces in addition to the carwash bay, as recommended by staff.*

20. **Tractor Trailer Loading Space:** Per TLZO Sec. 11.9, show the location of a semi-trailer loading space on the plat. Currently the plat only shows an SU-vehicle loading space. (*DPR Comment 29, Attachment 2*)

Applicant Response: *Sheet 1 has been revised to show the location of a semi-trailer loading space, as recommended by staff.*

21. **Sight Distance Suggestion:** Staff believes it would be useful to show sight distance profiles for the two proposed vehicular access points on Trimble Way with this application to ensure entrance locations are safe (these must be shown at time of the Final Site Plan). (Sheets 1-6) DCSM Sec. 10-120.2C(4). (*DPR Comment 26, Attachment 2*)

Applicant Response: *There are no sight distance requirements for private parking lot travel aisles.*

22. **Roadway Network Configuration:** On Sheet 2 the tanker truck turning movement analysis appears to have conflicts. At the entrance into the site from Trimble Plaza, the truck swings into the left lane in order to make a right turn and appears to clip the curb on the right hand side. Also the turn from access way at station onto Trimble Plaza appears to clip the curb. Review and revise as necessary to provide for safe maneuvering of the design vehicle per TLZO Sec. 3.4.6.E.7. (*DPR Comment 27, Attachment 2*)

Applicant Response: *Sheet 2 has been revised as recommended by staff. Sheet 3 has been added to show trash truck movements on the site.*

Stormwater Management

23. **Conceptual SWM Plan:** Provide some details of a proposed strategy and conceptual layout including preliminary calculations for meeting the requirements of a BMP "Hot Spot" on Sheet 1 per DCSM Sec. 10-120.2G(8). (*DPR Comment 12, Attachment 2*)

Applicant Response: *Sheets 7 and 8 have been added to the plat set to address these comments.*

24. **Storm Sewer Design:** The storm sewer layout shown does not appear to take into consideration the future development of adjoining lots and does not provide curb inlets at intersections and entrances. Staff recommends that the storm sewer layout be revised now to include the location of curb inlets, provide 90° crossings of roads, utilities and access ways and minimizing the number of manhole structures in the paved areas of the access ways, while still providing points of connection for BMP facilities and future development in order

to demonstrate that this can be accomplished without modifications on Sheet 5 per TLZO 3.4.14. (*DPR Comment 13, Attachment 2*)

Applicant Response: *Sheet 5 has been revised as recommended by staff taking future development into consideration.*

25. Adequate Outfall: The 344.5' invert of the proposed 30" RCP (at the downstream end of Ex. 20) does not appear to be feasible, using a minimum pipe slope of 0.50%, with the connection at the structure EX. 151. It appears to be too low to make the connection. Staff recommends that this be reviewed now to avoid problems at site plan review. (Sheet 5) TLZO 3.4.6.E.23. (*DPR Comment 13, Attachment 2*)

Applicant Response: *The elevation is now 352. It has been designed for Land Bay C and will work.*

26. Irrigation: Please clarify whether a lawn irrigation system is proposed for this use. (*Utilities Comment 12, Attachment 5*)

Applicant Response: *No irrigation is proposed for the service station site, but the landscaped area along Miller Drive does include an irrigation system.*

Issues at Time of Site Plan

The items below are included to give advance notice to the Applicant of issues normally addressed at the site plan review phase and are *not required* to be addressed at this time.

27. Use Limitation: This is just a reminder to the applicant that Note 11 on Sheet 1 of the approved Concept Plan for TLZM-2006-0002 states in part as follows:

According to Section 6A-17(5)(g) [now Sec. 8.6.5.F] . . . service stations . . . may be located in free-standing buildings; provided, however, that such uses shall be architecturally compatible with the adjacent buildings and shall not have frontage or direct access to a major or minor arterial street as defined in the adopted Town Plan. Such uses shall be an integral design element of an employment building complex of not less than 30,000 square feet of gross floor area and shall be allowed only in those locations shown on an approved final development plan.
(*Project Manager Comment, no attachment*)

Applicant Response: *Comment noted.*

28. Private Travelways: The applicant will need to provide the necessary public ingress-egress easements that would extend from curb to curb (back of sidewalk if present) along private streets and travelways. Alternatively, the applicant could provide a Business Owners Association document that addresses private easements for ingress/egress, parking, storm drainage, SWM/BMP and other privately maintained utilities (Sht. 1-5) per TLZO Sec. 3.4.6.E.1 and SLDR Sec. 2.04. (*DPR Comment 2, attachment 2*)

Applicant Response: *Comment noted.*

29. **Stormwater:** The applicant will need to provide a utility specific easement plat and standard Town deed of easement for the proposed BMP facility if applicable. Since this site is deem a "Hot Spot", please provide a BMP strategy and computation to demonstrate compliance with green space "Virgin land" 16% average land cover condition and the design shall comply with the new Virginia Stormwater Management Regulations and methodology as applicable per DCSM 5-620, 4VAC 50-60-48; 4VAC 50-60-63; and 4VAC-50-60-65. *(DPW Senior Engineer Comments, Attachment 3)*

Applicant Response: *Comment noted.*

30. **Sewer and Water:**

- a. The first waterline stub is shown as a cross. If the stub heading to the west is for the future lot it must be extended to a point beyond the pavement. *(Utilities Comment 3, Attachment 5)*

Applicant Response: *Comment noted.*

- b. The connection to sewer manhole "a" on sheet 5 can be achieved with a 4" or a 6" line with cleanouts in lieu of manhole. *(Utilities Comment 4, Attachment 5)*

Applicant Response: *Comment noted.*

- c. Due to the angle of sewer penetrating at manhole "a" the better solution will be to make this connection at manhole "S22". *(Utilities Comment 5, Attachment 5)*

Applicant Response: *Comment noted.*

- d. Show the location of proposed water service to the store and the car wash on the site plan. *(Utilities Comment 6, Attachment 5)*

Applicant Response: *Comment noted.*

- e. The angle of the crossing of 66" storm and the waterlines is unacceptable unless the waterline is above the storm sewer. *(Utilities Comment 7, Attachment 5)*

Applicant Response: *Comment noted.*

- f. If food preparation is proposed within the store a grease trap or interceptor will be required and must be shown or added as a note to the site plan. *(Utilities Comment 8, Attachment 5) [Conflict?]*

Applicant Response: *Comment noted.*

- g. Proposed use will require payment of water and sewer availability, pro-ratas and connection fees prior to issuance of a zoning permit. Contact Lisa Smith, Utility Permit Associate at 703-771-2762 for calculation of fees per Town Code Chapter 34. *(Utilities Comment 9, Attachment 5)*

Applicant Response: *Comment noted.*

h. Include the following note on the site plan:

Backflow prevention devices are required on all waterlines (domestic and fire) withdrawing water from the town's water system. If the water meter serving the building is larger than two inch, then the backflow prevention device shall be installed on the domestic service line within the mechanical room of the building served and must be accessible for testing, maintenance, and inspection. In addition, a backflow prevention device must be installed on all sprinkler lines between the town and building systems and must be accessible for testing, maintenance, and inspections. The device must be tested after installation and certificate of inspection provided to the town. Contact the Utilities Department's Environmental Compliance Officer at (703) 737-7592 for specific requirements (DCSM 2-122-4.D2 and Town Code Chapter 34, Division 3).

Applicant Response: Comment noted.

The applicant believes these responses adequately address staff's concerns, and we look forward to staff's response to this submission.

Sincerely,

WALSH, COLUCCI, LUBELEY, EMRICH &
WALSH, P.C.



Christine Gleckner, AICP
Land Use Planner

CEG/tlm
Enclosure

cc: Andy Shuckra, Keane Enterprises
Jack Williams, Paciulli, Simmons